

HEPBURN SPRINGS URBAN DESIGN FRAMEWORK

VOLUME ONE FRAMEWORK REPORT

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HEPBURN SPRINGS URBAN DESIGN FRAMEWORK

VOLUME 1

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VOLUME 2 comprises:

Part 1 - Analysis

- Introduction
- Context
- Review of Plans, Policies and Strategies
- Issues
- Analysis of the Study Area

Part 2 - Strategy Development

- Overview
- Land Use Patterns
- Movement Patterns
- Urban Form
- Implications for the Planning Scheme

Bibliography

Appendices

LIST OF ABBREVIATIONS USED IN THIS REPORT

ABC	Aerial Bundled Cabling
B1Z	Business 1 Zone
CFA	Country Fire Authority
CMA	Catchment Management Authority
DDO	Design and Development Overlay
DNRE	Department of Natural Resources and Environment
DOI	Department of Infrastructure
ERZ	Environmental Living Zone
ESO	Environmental Significance Overlay
HMSR	Hepburn Mineral Springs Reserve
HPS	Hepburn Planning Scheme
LDRZ	Low Density Residential Zone
LPPF	Local Planning Policy Framework
MFPC	Municipal Fire Prevention Committee
MFPP	Municipal Fire Prevention Plan
NCO	Neighbourhood Character Overlay
PCRZ	Public Conservation and Resource Zone
PPRZ	Public Purposes and Recreation Zone
PUZ	Public Use Zone
R1Z	Residential 1 Zone
RLZ	Rural Living Zone
RTDP	Regional Tourism Development Plan
RZ	Rural Zone
SLO	Significant Landscape Overlay
SPPF	State Planning Policy Framework
SUZ	Special Use Zone
UDF	Urban Design Framework
VCAT	Victorian Civil and Administrative Tribunal
VMWC	Victoria Mineral Water Committee
VPO	Vegetation Protection Overlay
WMO	Wildfire Management Overlay

NOTES

- Wherever reference is made to the "town" or "township" this means Hepburn Springs and Hepburn unless otherwise stated
- References to Hepburn Springs should be taken to include Hepburn Springs and Hepburn unless otherwise stated or the reference is clearly to one or the other

HEPBURN SPRINGS URBAN DESIGN FRAMEWORK

EXECUTIVE SUMMARY

Introduction

An Urban Design Framework is a broad concept plan(s) that provides a physical vision and a way of achieving local strategies and objectives. The Urban Design Framework is being placed on public exhibition prior to being considered by Council for adoption.

Urban Design Vision

- The following is the expression of a vision for the town for the next 10 to 15 years.

In future years Hepburn/Hepburn Springs will:

- *still enjoy its country 'village' atmosphere, human scale and treed environment.*
- *be able to absorb new development which is innovative and meets the guiding design principles of the Planning Scheme.*
- *demonstrate the value of best practice techniques of environmental management.*
- *be a safe and accessible town.*
- *be a vital and interesting town which retains its simple, streetscape character and in which commercial development does not dominate streetscapes.*
- *be a town in which elements of its cultural diversity and heritage are valued and expressed.*
- *have a viable population and local economy able to support the infrastructure and increased level of services to which the local community aspire.*
- *have an increased population through retention and growth of the existing population and attraction of new permanent inhabitants.*
- *be a popular tourist destination which emphasises the natural environment and resources associated with the mineral springs.*
- *have retained and enhanced a cultural and physical character which is distinct from Daylesford, but is complementary to the attributes of the Shire and the region.*

Recommendations

GROWTH AND DEVELOPMENT

- Adopt a 500 m² minimum lot size for residential development as provided for in ResCode, as an interim measure pending a more detailed analysis

- Carry out a viewshed analysis to determine sensitive areas where development could have an adverse impact on view quality
- Carry out a township character analysis to determine the significant elements of township character and to develop building and design guidelines
- Carry out a detailed analysis of township land by identifying and mapping constraints to development, and overlay the information to highlight areas of land that can most readily accommodate growth by infill.
- Review the policy statement of the Municipal Strategic Statement regarding the nature and extent of commercial uses in the town centre and support the policy with design guidelines.

FIRE SAFETY

- Build on public awareness and education programs of the Municipal Fire Prevention Plan regarding fire risk and safety standards by preparing information sheets that show how building and design guidelines specifically apply to Hepburn and Hepburn Springs
- Review mapping of Wildfire Management Overlay areas.

ENVIRONMENTAL MANAGEMENT

- Investigate models for a community consultative body to develop an effective management strategy for the gullies and reserves.
- Investigate the most appropriate planning scheme controls to create buffer zones at the interface between developed township land and the public reserves
- Seek to have the State policies in the Planning Scheme expanded to include water conservation objectives and policies and introduce local planning policies for water conservation
- Review the extent of the existing Environmental Significance Overlay 2 with respect to recharge areas for the mineral springs

PEDESTRIAN PATH NETWORK

- Construct or upgrade the following paths according to priority:
 - Existing paths on both sides of Main Road between Tenth Street and Fifth Street, and particularly those sections in poor condition
 - Existing path on south side of Tenth Street leading to the RSL Memorial gates
 - New asphalt footpath on the west side of Main Road between Sixteenth Street and Twentieth Street

- New concrete path to school frontage to ensure continuity of path along Main Road.
- New path along Newstead Road from Doctors Gully to Hepburn Tourist Road and Mineral Springs Crescent
- New path along Main Road and Mineral Springs Crescent from Tenth Street to Mineral Springs Reserve
- New shared path in Tenth Street from Doctors Gully to Main Road
- Develop a program for repair, upgrading and installation of paths in the reserves in conjunction with Parks Victoria, Victorian Mineral Water Committee and Department of Natural Resources and Environment, with priority given to ensuring that an effective network is provided
- Investigate possible improvements to the paths in the Hepburn Mineral Springs Reserve between Main Road / Tenth Street and Ninth Street and the Mineral Springs Reserve to reduce the steepness of the grade
- Emphasise the links from township streets to the trails in the reserves, by
 - upgrading of street footpath connections
 - resolving conflicts between vehicles and pedestrians at the entrances to the reserves,
 - directional signage
- Seek additional funding for extensions to the bicycle path along Main Road, especially where a shared path is proposed
- Give walking routes more publicity

BIKE PATHS

- Construct the following paths:
 - Upgrading of the path between Daylesford and Tenth Street
 - Extension of the path from Tenth Street to the Mineral Springs Reserve
 - Extension of the Main Road path from the intersection of Main Road and Mineral Springs Crescent to Twentieth Street
- Review the extent of trails through the public reserves that Parks Victoria and DNRE will allow to be used for mountain biking
- Install bicycle racks at key locations in the town

PUBLIC TRANSPORT AND TRAFFIC

- **Public Transport**
 - To increase the frequency of the bus service and extend the journey
 - Examine options for the conventional bus service that may provide a more flexible and responsive service.

• Traffic Issues

- Change to a 50km/hr speed limit in Main Road between Second and Fifteenth Street and in Mineral Springs Crescent/Forest Drive to Range Road.
- Alter the intersection of the Newstead Road and Hepburn Tourist Road to improve pedestrian safety for crossing the road, and to improve the appearance
- Upgrade Ajax Road/Fifteenth Street as a by-pass route.

• Parking

- Strictly enforce the Planning Scheme requirements for carparking in new development.
- Develop design and siting guidelines for carparking areas in new development.
- Ensure that streetscape works do not significantly reduce the amount of available kerbside parking.
- Provide long term parking for buses and large vehicles that does not require prime kerbside parking.

PREFERRED TOWNSHIP CHARACTER

- Prepare a detailed Township Character Strategy which:
 - establishes the broad character "theme" areas or precincts
 - carries out a detailed survey to establish precise boundaries and characteristics of precincts
 - proposes accompanying controls and policies including building siting and design guidelines
- Include the Urban Design Framework as a Referenced Document in the Planning Scheme.
- Adopt an Interim Policy for Township Character until a Township Character Strategy is adopted by Council.
- Produce an information pamphlet to increase awareness of design issues that are relevant to the township.

HERITAGE

- Review and update the Conservation/Heritage Study for the Shire, and amend the Planning Scheme accordingly.
- Prepare building siting and design guidelines for new development.
- Develop a heritage walking trail with signage and a guide pamphlet.
- Seek funding assistance to restore the swing bridge in Doctors Gully

VEGETATION

- Carry out a Landscape study to identify the extent and nature of important landscape elements
- Consider appropriate measures in the Planning Scheme to ensure protection of vegetation

VIEWS

- Carry out a detailed analysis of views and recommend policies and controls for the protection of views.

- Provide design guidelines for new development in areas where views could be degraded.
- Clear vegetation around Jacksons Lookout to improve views
- Improve views into Hepburn Mineral Springs Reserve from Main Road and Mineral Springs Crescent approach, by clearing vegetation on public land.

STREETSCAPES

- Develop a program of streetscape improvements with a common design theme, giving priority to Main Road.
- Use streetscape design to mark significant places and landmarks.
- Ensure that building form, scale, height and materials respect the character of the streetscape.

STREET TREE STRATEGY

- Prepare a detailed street tree planting plan for Main Road and carry out planting.
- Assess the opportunities for additional tree locations in secondary streets in the Core precinct and prepare a planting plan.
- Establish a program encouraging the community to plant street trees in the vicinity of their properties in accordance with the strategy.

PUBLIC OPEN SPACE

- Investigate potential availability of land (for acquisition, lease or dedication for public use) in Main Road in the Core precinct.
- Seek opportunities to incorporate sufficient open space for a small public park in new developments in the town centre.
- Investigate issues relating to the use of the Primary School grounds for general public use, and, if necessary, negotiate with the School Council for public access after school hours.

INFRASTRUCTURE

- **Overhead powerlines**
 - Place overhead cabling underground in Main Road and Mineral Springs Crescent between Fifth Street and the entry to Hepburn Mineral Springs Reserve.
 - Coordinate undergrounding with the Bike Path project and streetscape improvements.
- **Public Toilets**
 - Incorporate public toilets in a new public park within the town centre
 - If provision of a park is not achievable in the foreseeable future, include public toilets in the proposed plaza at Eighth Street.
- **Servicing of land**

Consider possible amendments to local policies in the Planning Scheme regarding the level of

servicing that should be required in Residential 1 land.

• Depot

- Investigate alternative sites for the Depot operation and the feasibility of relocation.
- Increase screen planting on all street frontages of the existing site.

• Street Furniture

- Provide new seating and other elements of street furniture to a new standard design throughout Main Road.

SIGNAGE

- Rationalise and install additional directional and information signage as part of a coordinated strategy
- Upgrade Gateway signage
- Include sign guidelines as a desired outcome of the Township Character Study

PUBLIC ART

- Establish a community art panel, including local artists and artisans, to formulate a public art strategy for Hepburn Springs, advise Council on potential projects and oversee implementation
- Seek expressions of interest from local artists and artisans for the design and provision of street furniture.

PLANNING SCHEME

- Review the Hepburn Planning Scheme provisions in the light of the recommendations of the Urban Design Framework
- Subject to the agreement of affected land owners, commence the procedure for minor rezoning of land as recommended.

Implementation

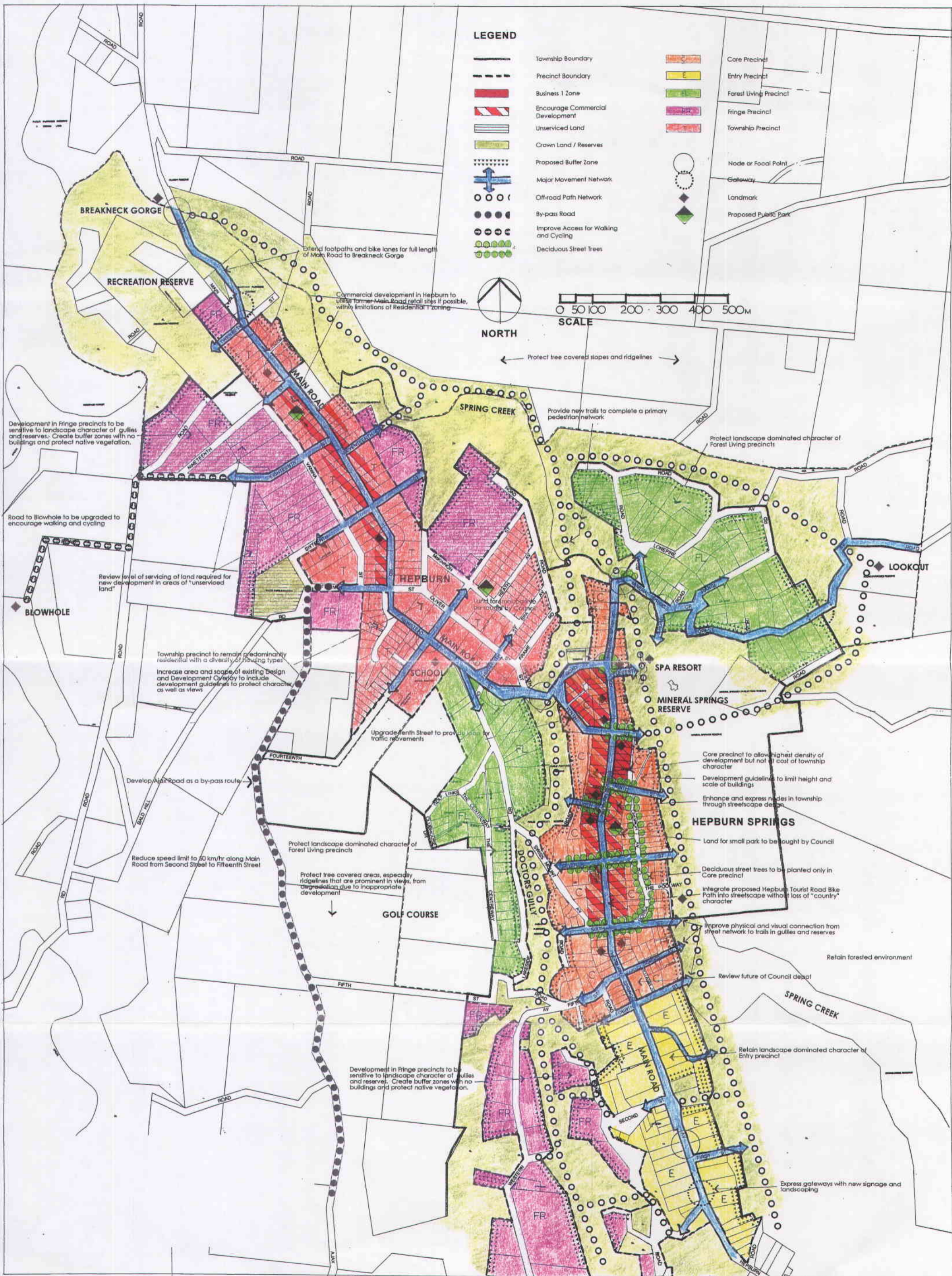
The above recommendations have been assembled into projects. Each project has been given a priority of VERY HIGH, HIGH, MEDIUM, or LONG TERM.

The greatest priority has been given to upgrading paths and the construction of the Tourist Road Bike Path, reducing the traffic speed limit, carrying out a Township Character Study and beautifying the Council depot site.

Contact

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This project was funded by the Victorian Government (Department of Infrastructure) and the Shire of Hepburn.



**HEPBURN SPRINGS URBAN DESIGN FRAMEWORK
COORDINATING FRAMEWORK PLAN**

Figure 1.1

HEPBURN SPRINGS URBAN DESIGN FRAMEWORK

PART 1 - FRAMEWORK REPORT

□ OVERVIEW

The Urban Design Framework comprises two volumes:

- Volume 1 Framework Report
- Volume 2 Supporting Report.

This Volume 1 contains all of the main study recommendations, the coordinating framework plan, concept plans for specific streetscape projects and a preliminary program for implementing projects and recommendations.

Volume 2 contains the analytical studies and discussion of strategies and options. Additional background information, including records of community consultation, are appended to this volume.

The Framework Report commences with a restatement of key issues and the urban design principles established during the community consultation process.

□ KEY ISSUES

The public consultation program established that the major issues of concern to the community were:

- Need to improve pedestrian safety
- Desire to encourage walking rather than driving
- Limitations on growth
- Desire to retain and enhance views
- Intrusive nature of powerlines in Main Road
- Fire safety
- Need to improve environmental quality of gullies and reserves
- Threats to township character
- Need for good (and effective) urban planning
- Threats to agricultural land
- Desire to retain country feel in streetscape
- Need to improve user facilities and amenities in main street - focal point, toilets, seating etc.
- Poor signage
- Maintenance of cultural and economic diversity of residents
- Proposed bike path
- Town's role in relation to mineral springs

□ URBAN DESIGN PRINCIPLES

The recorded issues and analysis of public comment has informed the establishment of the following urban design principles which drive the strategies and actions recommended in the Urban Design Framework:

- Encourage growth in the town core
- Maintain country 'village' character
- New development must respect the landscape quality that is essential to the character and appeal of Hepburn.
- Views within town and from key locations to/from the surrounds that are an intrinsic element of township character must be considered in the design of new development.

- Differences in urban character within the township should be recognised and maintained where they reflect the diversity in the cultural identity, landscape and function of the town.
- Protect and express sites and elements of environmental and heritage value.
- Innovation and design excellence in development is to be encouraged.
- Private development should contribute positively to the public domain and the vitality and diversity of experiences in Hepburn Springs.
- New development should demonstrate best practice techniques in managing environmental issues, including energy use, waste and other threats to ecological sustainability.
- Encourage new development in areas that are well serviced by existing infrastructure..

Urban Design Vision

An important component of an Urban Design Framework is the expression of a vision for the town which extends over a period of, say, 10 to 15 years. The following is the consultant's interpretation of community values, and the town's character and potential.

In future years Hepburn/Hepburn Springs will:

- *still enjoy its country 'village' atmosphere, human scale and treed environment.*
- *be able to absorb new development which is innovative and meets the guiding design principles of the Planning Scheme.*
- *demonstrate the value of best practice techniques of environmental management.*
- *be a safe and accessible town.*
- *be a vital and interesting town which retains its simple, streetscape character and in which commercial development does not dominate streetscapes.*
- *be a town in which elements of its cultural diversity and heritage are valued and expressed.*
- *have a viable population and local economy able to support the infrastructure and increased level of services to which the local community aspire.*
- *have an increased population through retention and growth of the existing population and attraction of new permanent inhabitants.*
- *be a popular tourist destination which emphasises the natural environment and resources associated with the mineral springs.*
- *have retained and enhanced a cultural and physical character which is distinct from Daylesford, but is complementary to the attributes of the Shire and the region.*

□ SUMMARY OF RECOMMENDATIONS

The following are the main findings and recommendations to emerge from the Urban Design Framework study. They are presented under the same headings as the Strategies and Actions in Volume 2 of the report.

Detailed explanation of proposed actions have been omitted, for the sake of simplicity. Further details can be obtained by referring to Volume 2.

Priorities have been assigned to each recommendation and these appear in the Summary of Projects in the IMPLEMENTATION section later in this report.

GROWTH AND DEVELOPMENT

Main findings

Discussion about growth in the township has focussed on two areas:

- (1) concern about growth in tourism and the consequent effects of new development
- (2) appropriateness of design of new development.

The MSS and Local Policies aim to discourage any growth outside the township boundaries, and incorporate objectives intended to direct growth to preferred areas in a sensitive manner. However, detailed analysis of growth opportunities and the essential character of landscape and built environment will provide the basis for using more sophisticated tools to assess development applications.

Clearer policies about the nature and location of commercial development would support community values.

Recommendations

- Adopt a 500 m² minimum lot size for residential development (without requiring a planning permit) as provided for in ResCode, as an interim measure until a more detailed analysis can be completed
- Carry out a detailed land suitability analysis of the township with respect to:
 - extent of infrastructure services (sewer, stormwater, electricity, gas and water)
 - land slope and soil types
 - sensitive vegetation (visual quality and environmental value)
 - proximity to watercourses
 - proximity to spring eyes and bores
 - recharge areas for mineral water and groundwater resources
 - accessibility from existing made roads
 - fire risk
 - township character
 - views and viewshed, and
 - other relevant factors in order to:
 - (1) highlight areas of land that can most readily accommodate growth by infill.
 - (2) develop building and design guidelines that will encourage sensitive development
- Review the policy statements of the MSS regarding the nature and extent of commercial uses in the town centre to indicate more specific locations (including Hepburn) and to support the policy with building siting and design guidelines.

FIRE SAFETY

Main findings

Public concern about fire safety was largely related to the condition of the gullies (see Environmental Management recommendations). However, the consultants believe that there are potential conflicts between the recommended methods of managing native vegetation on developed land in wildfire-prone areas and objectives for retention of vegetation for its environmental value and landscape character.

The MFPP partly addresses this issue with regard to education and awareness programs, and existing standards for building siting and design are available. The township analysis recommended above will assist the preparation of guidelines that are specific to Hepburn Springs.

Recommendations

- Build on the programs of the MFPP with regard to public awareness and education about fire risk and safety standards by preparing information sheets that show how building and design guidelines specifically apply to Hepburn and Hepburn Springs
- Review the mapping of WMO areas.

ENVIRONMENTAL MANAGEMENT

Main findings

Disparate views and conflicts have led to a disappointing level of satisfaction with the management of the gullies and reserves surrounding the town, particularly with regard to fire risk and weed infestation. A community-led strategy has the best chance of being accepted and implemented.

The MSS expresses the need for sensitive development of the interface of the urban areas and the adjacent reserves. Specific guidelines established through an overlay or similar control in the Planning Scheme would strengthen these objectives and provide development guidelines.

In the Planning scheme the need to manage stormwater run-off is addressed and catchment areas are protected. However, there are no specific policies or guidelines for the conservation of on-site water resources.

The Planning Scheme does not protect all identified recharge areas for the mineral springs.

Recommendations

- Investigate models for a community consultative body to develop an effective management strategy for the gullies and reserves that is accepted by the community and the relevant authorities and agencies. The strategy should address:
 - pest plant control
 - fire risk from indigenous plant species and density of vegetation
 - fire access
 - wildlife habitat and corridors
 - desired character
 - historical aspects of land use in the gullies
 - implementation programs, including community volunteering
 - education and awareness programs
 - sources of funding
- Investigate the most appropriate planning scheme controls to create buffer zones at the interface between developed township land and the public reserves
- Seek to have the SPPF policies expanded to include water conservation objectives and policies and introduce local planning policies for water conservation
- Review the extent of the existing Environmental Significance Overlay 2 with respect to recharge areas for the mineral springs

PEDESTRIAN PATH NETWORK

Main findings

The township is well served by walking trails in the adjacent reserves, but street footpaths are often in poor condition or non-existent. Both types of path have missing links.

The paths also need upgrading to meet current day standards for access and mobility.

Recommendations

- Construct or upgrade the following paths as a priority :
 - ◊ Existing footpaths on both sides of Main Road between Tenth Street and Fifth Street, and particularly those sections in poor condition
 - ◊ Existing footpath on south side of Tenth Street leading to the RSL Memorial gates
 - ◊ New asphalt footpath on the west side of Main Road between Sixteenth Street and Twentieth Street
 - ◊ New shared path to school frontage to ensure continuity of path along Main Road (including upgrading and widening of path from Tenth Street to an asphalt path).
 - ◊ New shared path along Newstead Road from Doctors Gully to Hepburn Tourist Road and Mineral Springs Crescent
 - ◊ New footpath along Main Road and Mineral Springs Crescent from Tenth Street to Mineral Springs Reserve
 - ◊ New shared path in Tenth Street between Doctors Gully and Main Road.
- Develop a program for repair, upgrading and installation of walking trails in the reserves in conjunction with Parks Victoria, VMWC and DNRE with priority given to ensuring that an effective network is provided.
- Incorporate ramped kerb crossings, tactile pavers and other aids to use of paths
- Investigate possible improvements to the paths in the HMSR between Main Road / Tenth Street and Ninth Street and the Mineral Springs Reserve to reduce the steepness of the grade to the limits acceptable in AS 1428 or other feasible standards
- Emphasise the links from township streets to the trails in the reserves, by
 - ◊ upgrading of street footpath connections to the trails and infilling gaps in the continuity of paths
 - ◊ resolving conflicts between vehicles and pedestrians at the entrances to the reserves, particularly where unrestricted vehicle access is possible
 - ◊ directional signage
- Provide more publicity and information about walking routes

BIKE PATHS

Main findings

The brief for the Urban Design Framework required that the proposals for the Hepburn Tourist Road bike path be integrated with the streetscape proposals. This project now has VicRoads funding. The Concept Plans included in the Urban Design Framework show that some changes in the streetscape will be necessary to meet VicRoads standards. The final design needs to be approved by VicRoads and acceptable to the community.

The bike path only extends as far as Tenth Street and needs to provide safe access for cyclists to the Hepburn Mineral Springs Reserve and the school at the very least.

Paths or trails through the reserves can be used by mountain bikes, but there need to be some linkages completed to provide a comprehensive network. The highest priority should be given to an alternative route to/from Daylesford.

Recommendations

- Negotiate with VicRoads and the community to determine the most acceptable design for the upgrading of the shared path and on-road lanes between Daylesford and Tenth Street
- Construct the following bicycle paths and on-road lanes as a priority:
 - ◊ On-road lanes from Tenth Street to the Mineral Springs Reserve
 - ◊ Main Road shared path from the intersection of Main Road and Mineral Springs Crescent to Fourteenth Street
 - ◊ On-road lanes from Fourteenth Street to Twentieth Street
 - ◊ Off-road shared path from Twentieth Street to Breakneck Gorge
 - ◊ Off-road shared path in Tenth Street between Doctors Gully and Main Road.
- Seek additional funding for extensions to the bicycle path along Main Road, especially where a shared path is proposed to provide continuity of the pedestrian and bicycle path network (for instance, through Doctors Gully cutting to the school)
- Review the extent of trails through the public reserves that Parks Victoria and DNRE will allow to be used for mountain biking with a view to increasing connectivity of the network
- Install bicycle racks at key locations in the town

PUBLIC TRANSPORT AND TRAFFIC

Main findings

The only public transport service is an infrequent and inflexible bus which has limited destinations within the township. The local community (and particularly the young and the elderly) and visitors would all benefit from a more frequent, flexible and comprehensive service.

Growth in traffic(both volume and speed) is a concern to the community. Measures to reduce volume such as a by-pass for through traffic would be compatible with a reduction in the speed limit. Both would improve pedestrian safety and general amenity and allow some streetscape improvements.

Kerbside parking is seen to be at a premium in the centre of the town and any reduction should be avoided.

Recommendations

PUBLIC TRANSPORT

- Increase the frequency of the bus service and extend the journey to the Mineral Springs Reserve and, say, Nineteenth Street on all journeys.
- Investigate alternatives to the conventional bus service that may provide a more flexible and responsive service.

TRAFFIC ISSUES

- Upgrade Ajax Road/Fifteenth Street as a by-pass route.
- Change to the default 50km/hr speed limit in Main Road between Second and Fifteenth Street and in Mineral Springs Crescent/Forest Drive from Main Road to Range Road.
- Alter the intersection of the Newstead Road and Hepburn Tourist Road to improve safety for pedestrians crossing the road (and also improve the appearance)

PARKING

- Strictly enforce the Planning Scheme requirements for carparking in new development. (Do not allow concessions for available kerbside parking.)
- Develop design and siting guidelines for carparking areas in new development.
- Ensure that streetscape works do not significantly reduce the amount of available kerbside parking.
- Provide long term parking for buses and large vehicles that does not require prime kerbside parking.

PREFERRED TOWNSHIP CHARACTER

Main findings

Maintenance of township character was an important issue for the community, particularly in light of some recent developments. The Planning Scheme includes broad objectives aimed at protecting character but does not provide adequate tools to enable thorough assessment of development applications. The recent introduction of ResCode provisions into the Planning Scheme has added impetus to the need for a Township (or Neighbourhood) Character Strategy and the preparation of development guidelines.

The Urban Design Framework has established a preliminary basis for assessment by identifying broad character precincts and inclusion of the Urban Design Framework in the Planning Scheme as a Referenced Document, will give it some legitimacy, as an interim policy.

Recommendations

- Prepare a detailed Township Character Strategy which:
 - establishes the broad character "theme" areas or precincts
 - carries out a detailed survey to establish precise boundaries and characteristics of precincts
 - proposes accompanying controls and policies, including building siting and design guidelines
- Include the Urban Design Framework as a Referenced Document in the Planning Scheme to provide an Interim Policy until a Township Character Strategy is adopted by Council.
- Produce an information pamphlet to increase awareness of design issues that are relevant to the township.

HERITAGE

Main findings

In two years' time the Council proposes to review the various conservation and heritage studies for the Shire. They have been completed over the last 15-20 years, generally by the former Shire Councils prior to amalgamation. It is important that this objective is achieved.

The heritage of Hepburn Springs is not very extensively celebrated in available tourist information, nor interpreted in the landscape. More publicity and visible interpretation would increase the town's appeal and community pride.

Recommendations

- Review and update the Conservation/Heritage Study for the Shire, and amend the Planning Scheme accordingly.
- Prepare building siting and design guidelines for new development.
- Develop streetscape designs that give prominence to landmark heritage buildings.
- Restore the swing bridge in Doctors Gully (in conjunction with Parks Victoria and subject to funding).

VEGETATION

Main findings

The topography and landscape of the township and its setting are major elements of its character and valued by its community. This is recognised in various ways in the Planning Scheme, but is not specifically defined. A properly constituted study will provide the research needed to incorporate additional controls to protect significant landscapes from degradation.

Recommendations

- Carry out a Landscape study to identify the extent and nature of important landscape elements, including recommendations for the most appropriate Planning Scheme provisions to ensure protection of vegetation.

VIEWS

Main findings

Closely linked to the landscape study is an analysis of important views. A preliminary analysis is included in the Urban Design Framework and could be used on an interim basis. However, the brief for the Urban Design Framework did not allow a sufficiently detailed study to determine the extent of the viewshed. Both the landscape study and the viewshed analysis could be combined with a Township Character Study, or done separately. A combined study would be more efficient.

Some clearance works on public land could improve existing views.

Recommendations

- Carry out a detailed analysis of views and the extent of the viewshed that is relevant to the town.
- Prepare policies and controls (which aim to protect views) to be incorporated into the Planning Scheme.
- Provide design guidelines for new development in areas where views could be degraded and locations where views and vistas may be obscured.
- Clear vegetation around Jacksons Lookout to improve views
- Improve views into HMSR from Main Road and the Mineral Springs Crescent, by clearing vegetation on public land.

STREETSCAPES

Main findings

The design and construction of the Hepburn Tourist Road Bike Path is a catalyst for upgrading the streetscape of Main Road. A comprehensive streetscape design can conform to a theme based on the town's mineral water heritage and similar to design themes already recommended for the Mineral Springs Reserve. It will include street trees, street furniture, signage and paving. Street lighting will also be renewed if existing services are placed underground. Landmarks and features of the town can be emphasised by the design.

Recommendations

- Develop a program of streetscape improvements with a common design theme, giving priority to Main Road.
- Mark significant places and landmarks by ensuring that street trees, special paving, artworks and other elements enhance the appearance and amenity of the place without obscuring views of facades, natural features and landscaping.
- Provide new seating and other elements of street furniture to a new standard design throughout Main Road.
- Incorporate criteria in building siting and design guidelines that ensure that building form, scale, height and materials respect the character of the streetscape.

STREET TREE STRATEGY

Main findings

Street tree planting is quite limited throughout the town. Whilst the community is concerned that street trees could obscure valued views and vistas, there is still scope for planting in many streets. Street trees will enhance the appearance of the town and reinforce its character.

Recommendations

- Prepare a detailed streetscape plan for Main Road (including street tree planting) and carry out planting.
- Assess the opportunities for additional tree locations in secondary streets in the Core precinct and prepare planting plans.
- Establish a program encouraging the community to plant street trees in the vicinity of their properties in accordance with the strategy.

PUBLIC OPEN SPACE

Main findings

There is a lack of small public parks in the central areas of the town, which could provide casual socialising and children's play opportunities. Unfortunately there is no public land in the most appropriate locations. A number of potential sites for park facilities have been considered and some are suitable, including the primary school, but ideally, land in the town centre will be acquired for a park.

The reserves and gullies are not considered suitable by parents due to the distance from houses and safety and security concerns.

Recommendations

- Investigate potential availability of land for a small park (for acquisition, lease or dedication for public use) in Main Road in the town centre (Core precinct).
- Seek opportunities to incorporate sufficient open space in new developments in or near the preferred locations.
- Investigate issues relating to the use of the Primary School grounds for general public use, and, if necessary, negotiate with the School Council for public access after school hours.

INFRASTRUCTURE

Overhead powerlines

Main findings

Overhead cabling has a major negative impact on streetscape appearance and the quality of views. Ideally, cabling would be placed underground for the full length of Main Road, however, the cost is prohibitive, except over a very long period. The recommendations focus on addressing the principal section of Main Road and the approach to the Mineral Springs Reserve.

Due to a lack of immediately available funds, undergrounding will not be done at the time of constructing the Main Road bike path and upgrading the streetscape. Nevertheless, it should remain a high priority project.

Recommendations

- Place overhead cabling underground in Main Road and Mineral Springs Crescent between Fifth Street and the entry to Hepburn Mineral Springs Reserve.
- Seek funding from the Victorian Government Powerlines Relocation Committee for any undergrounding project.
- Coordinate undergrounding with Bike Path projects and streetscape improvements, if possible.

Public Toilets

Main findings

The toilets in the Mineral Springs Reserve are the nearest in the town to the commercial centre. Toilets once existed in Tenth Street but have been removed.

The best location is in Main Road between Seventh and Tenth Street, but apart from using part of an existing road reserve, there is no public land that could provide a site. Toilets could be included in the proposed park in Main Road.

Recommendations

- Incorporate public toilets in a new public park within the town centre (see previous section).
- Include public toilets in the proposed plaza at Eighth Street if provision of a park is not achievable in the foreseeable future,

Servicing of land

Main findings

The MSS has noted that some residential land in Hepburn Springs is not serviced and that possible low density infill development should be investigated. All land in the town is required to meet tests for land suitability (particularly if it is not sewered) before it can be subdivided or developed. This is to protect the catchment area for the domestic water supply.

Best practice standards would suggest that new development on these lots should be connected to the existing sewerage reticulation and stormwater drainage systems should discharge without causing environmental degradation, rather than allowing on-site treatment of effluent.

Recommendation

- Include in the proposed land suitability analysis, consideration of possible amendments to the local policies in the Planning Scheme regarding the level of servicing that should be required in Residential 1 land.

Depot

Community concern that the Depot operation is not an appropriate use of residential land, looks unattractive and possibly contributes to pollution, suggests that other sites should be investigated. Short-term action can be taken to improve the appearance of the site.

Recommendations

- Investigate alternative sites for the Depot operation and the feasibility of relocation.
- Increase screen planting on all street frontages of the site.

SIGNAGE

Main findings

Signage in the town could be improved by rationalising the number and location of signs to provide coordinated, legible information.

Private signage, whilst not extensive, could be improved. It should comply with Council's existing policies, which, if necessary, could be supported by including signage in new building siting and design guidelines.

Recommendations

- Rationalise directional and informative signage as part of a coordinated strategy.
- Install new signage as part of Gateway improvements.

PUBLIC ART

Main findings

Public art is not prevalent in Hepburn Springs. If the community is willing to support a program of public art, experience in other communities shows that it enhances the social and cultural life of the community. It also reinforces local identity and improves appearance and interest in the public domain. A panel including Council representatives and local artists and artisans could assist Council in developing public art policy and projects.

Recommendations.

- Establish a community art panel, including local artists and artisans, to formulate a public art strategy for Hepburn Springs, advise Council on potential projects and oversee implementation.
- Seek expressions of interest from local artists and artisans for the design and provision of either a complete range of street furniture or individual pieces at key locations.

PLANNING SCHEME

Main findings

The recommendations of the Urban Design Framework will require that various aspects of the Planning Scheme be reviewed. The MSS is due to be reviewed in 2004, and much of the critical review should be carried out in time for the outcomes to be included in the revised MSS.

The following recommendations have not been included in any of the above thematic categories.

Recommendations

Subject to the agreement of property owners, commence the processes for the following rezonings:

- ◊ Golf Course land from PUZ7 to SUZ
- ◊ Crown land in Doctors Gully from ERZ to PPRZ
- ◊ Crown land east of the township (adjacent to Spring Creek from RUZ to PPRZ).

□ COORDINATING FRAMEWORK

The Coordinating Framework Plan (Figure 1.1) incorporates the key recommendations on one map for the township. These include:

Land Use

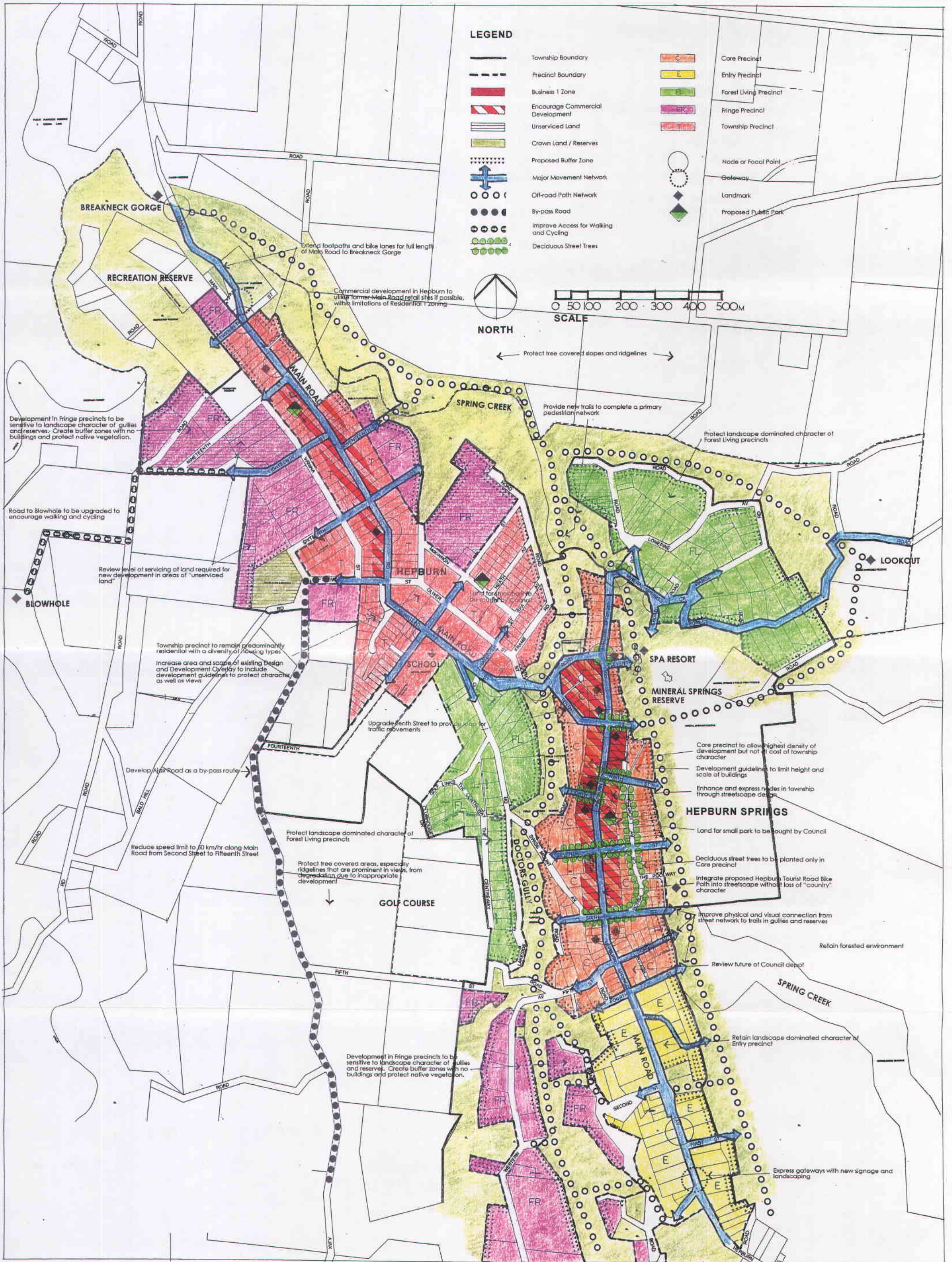
- extent of township
- preferred areas for commercial development
- public land
- unserviced land.

Movement Patterns

- traffic
- pedestrian network - priority linkages
- bicycle network - priority linkages

Built Form and Environment

- Township Character Precincts
- street tree character
- significant vegetation
- nodes and focal points
- gateways
- landmarks
- preferred locations for new public parks
- proposed buffer zones.



**HEPBURN SPRINGS URBAN DESIGN FRAMEWORK
COORDINATING FRAMEWORK PLAN**

Figure 1.1

HEPBURN SPRINGS URBAN DESIGN FRAMEWORK

PART 2 - ACTION AREA CONCEPT PLANS

□ IDENTIFIED PROJECTS

The following areas have emerged as important projects and conceptual designs have been prepared for each:

- Main Road Streetscape (incorporating proposed Hepburn Tourist Road Bike Path)
- Tenth Street intersection
- Eighth Street plaza.

In addition to these Concept plans, detailed proposals have been prepared for:

Footpaths

Signage - gateways
 - directional signs

Street furniture.

These proposals are conceptual. Final detailed proposals will be made available for further public comment as each project is developed.

□ MAIN ROAD STREETScape

RATIONALE

The urban design concept for Hepburn Springs and Hepburn is deliberately simple and low key. This reflects the community desire to retain the character of a country town and for Hepburn Springs and Hepburn to be visually different from Daylesford.

The plan proposes minimum change, concentrating on the replacement and enhancement of existing infrastructure, whilst incorporating minor improvements in the public domain.

KEY ELEMENTS

Key elements of the concept are:

- Undergrounding of powerlines between Fifth Street and the entrance to the Mineral Springs Reserve. (May need to be deferred until funding is available)
- Integration of the proposed bike path from Daylesford to Hepburn Springs and extension of bike paths to the Mineral Springs Reserve and to Breakneck Gorge.
- Replacement of existing footpaths with a new theme design and incorporating details to meet current standards for access and mobility.
- Extension of the path network to provide improved access to community facilities and between township attractions.
- Introduction of street tree planting to enhance streetscapes and to express variations in urban character within the township.
- Creation of small meeting places in the streetscape, especially at the Tenth Street intersection and at Eighth Street.
- Retention of important viewlines.
- Beautification of the exterior of the Council Depot in Main Road, (long term objective to relocate the depot to another site).
- Introduction of a coordinated signage strategy to reflect common themes and to reduce visual clutter.
- Alterations to the intersection of Newstead Road and Hepburn Tourist Road.

DESIGN THEME

The Mineral Springs Reserve Masterplan Review recommends the creation of a boulevard between Daylesford and Hepburn Springs, created by tree planting and supported by the repetition of a number of thematic elements, including water, sculpture, rock/stone and low level planting. The design theme for Hepburn Springs and Hepburn will be compatible with the recommendations of the Masterplan Review, however, the concept of an avenue of street trees within Hepburn Springs is at odds with the views expressed by the local community.

In Main Road, Hepburn Springs, therefore, avenue trees have been avoided in order to enable viewlines to be maintained, resulting in only a few deciduous trees being part of the Main Road streetscape at points where side roads intersect with Main Road. The use of deciduous trees in the streetscape in the Core Precinct is supported, but only in the side streets. This Urban Design framework deliberately creates a break in the continuity of the avenue of trees from Daylesford by encouraging informal native roadside vegetation within the Entry Precinct to make the distinction between Daylesford and Hepburn Springs.

Instead of planting an avenue of street trees in Main Road in the Core precinct, it is proposed to introduce low shrub planting at various points to beautify the streetscape. This will also relieve the monotony of the linear views, which will otherwise be defined at ground level by the road line markings, kerbs and channels and footpaths. Plant selection for these areas will be mainly lavender, which is hardy and easy to manage and reflects local attractions. Other plants could include native grasses (not in Core Precinct), Correa, Native Wax Flower (*Eriostemon*), *Westringia*, Rosemary, *Diosma*, *Grevilleas*, Box hedges and prostrate Junipers.

A number of nodes, focal points and landmarks have been identified in the Urban Design Framework. These will be highlighted in the streetscape design by a number of techniques:

- using street trees to mark the place with a formal layout which interrupts the flow of the streetscape space.
- using street trees to frame views of historic and notable buildings, without obscuring them.
- using low-level landscaping, variations in paving design and other smaller elements, such as street furniture, to draw attention to the place.
- incorporating appropriate signage
- installing feature street lighting
- considering the potential for incorporating public art.

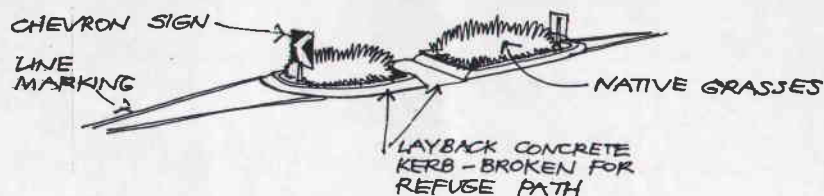
Some nodes are notable only because they are key intersections and these will be marked mainly by street tree planting, low level landscaping and signage. Important buildings, such as the Macaroni Factory, the Palais, Springs Hotel, Villa Parma and the Pioneer Bakery could be marked by more extensive detailing. The final designs for these parts of the streetscape will be subject to further consultation, particularly with the property owners. Care must be taken with the final design to ensure that it retains the "country feel" of the existing streetscape.

Most of the thematic elements to be used in the streetscape will be obvious only at the cyclist/pedestrian level, as the scale of the elements and speed of travel will deny motorists gaining a full impression of the theme design. Notwithstanding, signage along the road and elements such as street lighting, will repeat sufficient themes to leave an impression. Further details of the proposed streetscape elements appear later in this section.

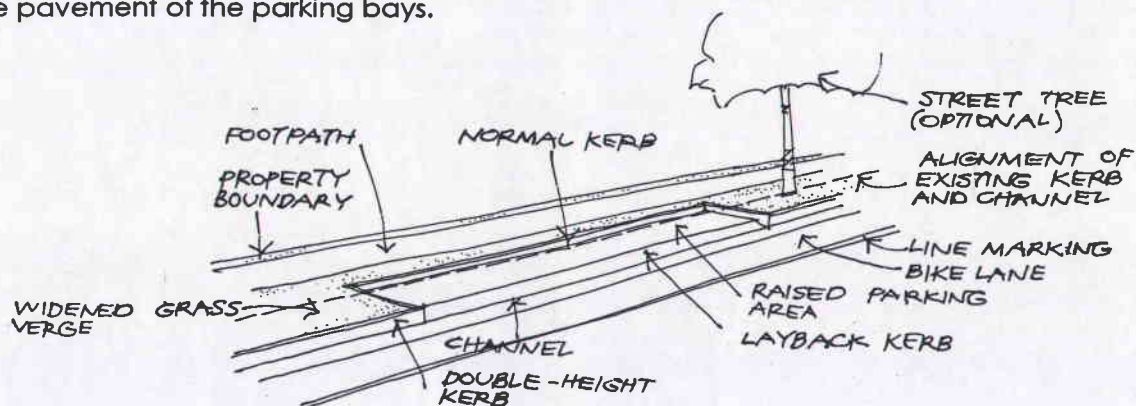
PROPOSED BIKE PATH

The Main Road Streetscape incorporates the proposed Hepburn Tourist Road Bike Path to the standards required by VicRoads. The ramifications of these standards are discussed in Volume 1. The following points are highlighted:

- A crossover point is proposed near Second Street. These will include a pedestrian refuge in the middle of the road, which should be constructed to the most simple detailing possible within road design standards. It is desirable that the refuge is planted with indigenous plants, such as native grasses and that signage, barriers, etc. are kept to an absolute minimum. The design should recognise the proposed 50km/hr speed limit.



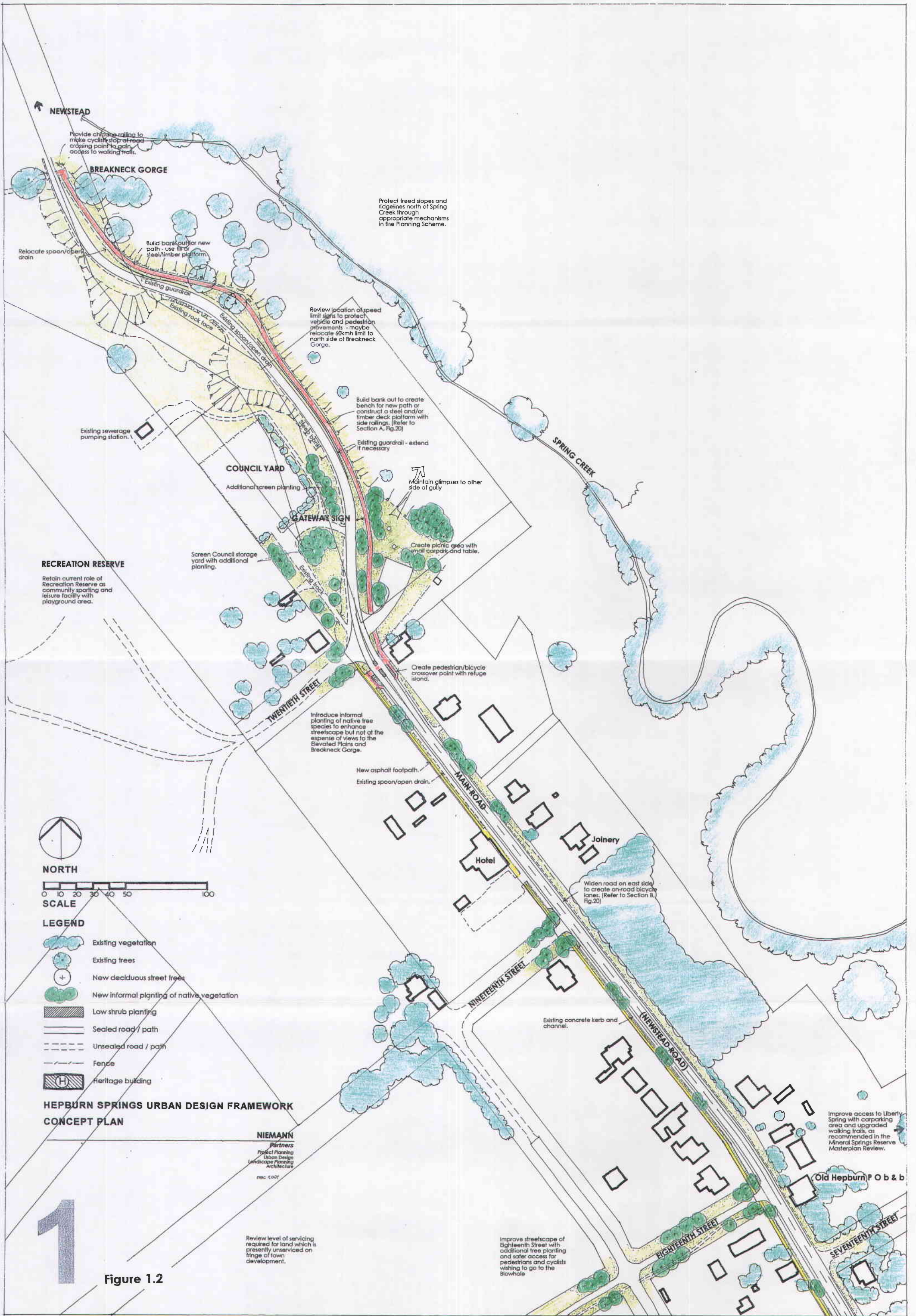
- Between Sixth and Tenth Street, VicRoads require a kerb-to-kerb width of 13m. minimum. To achieve this, the existing kerbline (mostly on the eastern side) will be relocated in some sections. The existing difference of level between the roadway and footpath in some cases will be too great for a normal kerb to be used without creating an excessively steep cross fall in the footpath. The kerb will need to be higher, preventing safe pedestrian access between parked vehicles and the footpath. Alternatively, kerbside parking could be eliminated. Another option (subject to confirmation of existing levels) is to realign the kerb and channel to the outer edge of the parking bays, adjacent to the bike lane, and elevate the pavement of the parking bays.

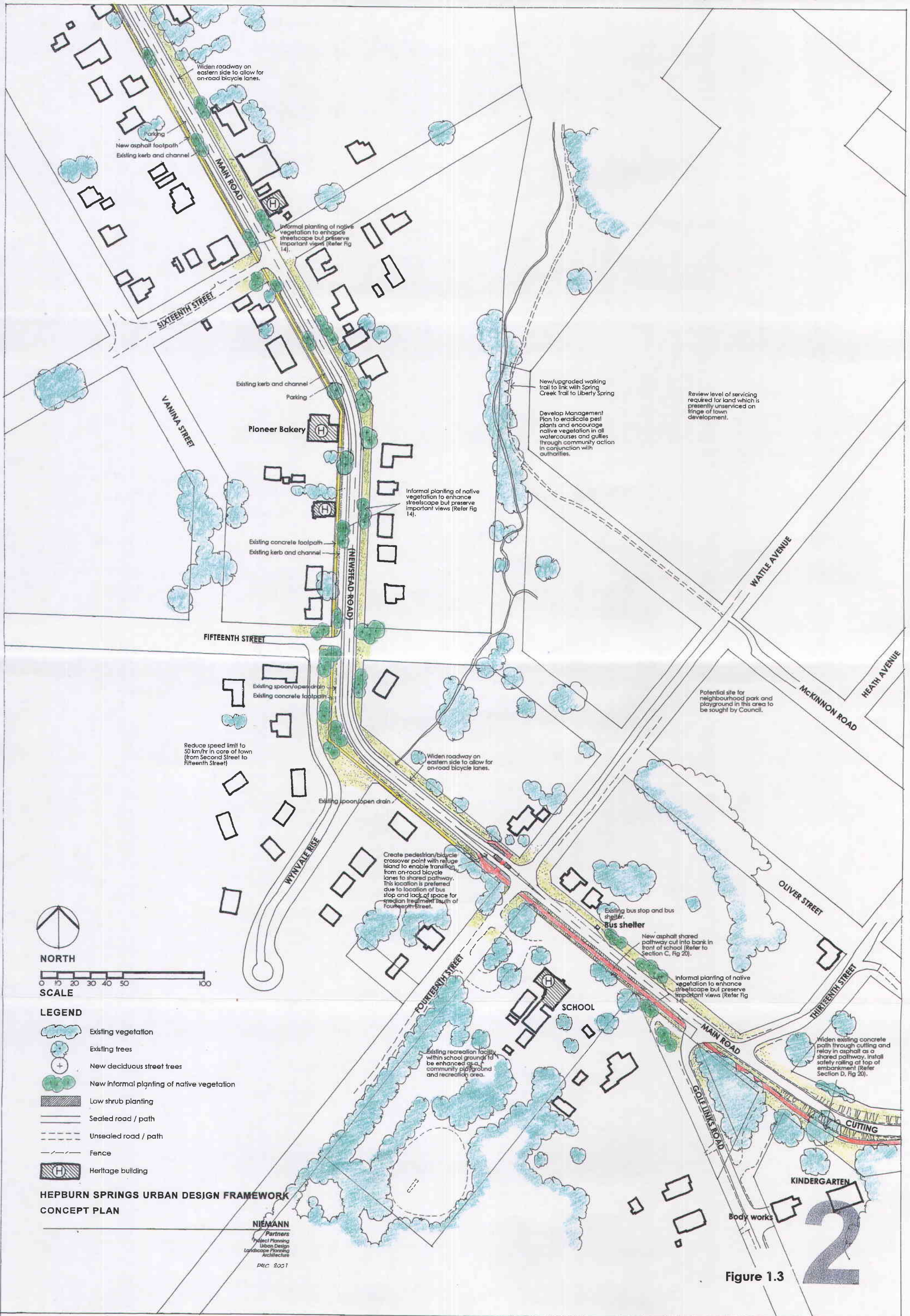


- Relocation of the kerbline on the western side is limited by existing electricity supply poles. Even if these were removed by undergrounding the service, the kerbline alignment should remain so that adequate space is available for the footpath, grassed verge, shop verandahs, etc.

□ CONCEPT PLANS

The concept plans for the whole of the Main Road streetscape (Figures 1.2 to 1.6 on the following pages) include other proposals for works in adjacent areas.





Widen roadway on eastern side to allow for on-road bicycle lanes.

Parking
New asphalt footpath
Existing kerb and channel

Informal planting of native vegetation to enhance streetscape but preserve important views (Refer Fig 14).

Existing kerb and channel
Parking

Pioneer Bakery

Informal planting of native vegetation to enhance streetscape but preserve important views (Refer Fig 14).

Existing concrete footpath
Existing kerb and channel

FIFTEENTH STREET

Existing spoon/open drain
Existing concrete footpath

Reduce speed limit to 50 km/hr in core of town (from Second Street to Fifteenth Street)

Widen roadway on eastern side to allow for on-road bicycle lanes.

Existing spoon/open drain

Create pedestrian/bicycle crossover point with refuge island to enable transition from on-road bicycle lanes to shared pathway. This location is preferred due to location of bus stop and lack of space for median treatment south of Fourteenth Street.

Existing bus stop and bus shelter
Bus shelter

New asphalt shared pathway cut into bank in front of school (Refer to Section C, Fig 20).

Informal planting of native vegetation to enhance streetscape but preserve important views (Refer Fig 14).

Existing recreation facility within school grounds to be enhanced as a community playground and recreation area.

Widen existing concrete path through cutting and relay in asphalt as a shared pathway. Install safety railing at top of embankment (Refer Section D, Fig 20).

WATTLE AVENUE

KINDERGARTEN

Body works

Review level of servicing required for land which is presently unserviced on fringe of town development.

Develop Management Plan to eradicate pest plants and encourage native vegetation in all watercourses and gullies through community action in conjunction with authorities.

New/upgraded walking trail to link with Spring Creek Trail to Liberty Spring

Potential site for neighbourhood park and playground in this area to be sought by Council.

MCKINNON ROAD

HEATH AVENUE

OLIVER STREET

THIRTEENTH STREET

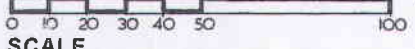
MAIN ROAD

GOLF LINKS ROAD

CUTTING



NORTH



SCALE

LEGEND

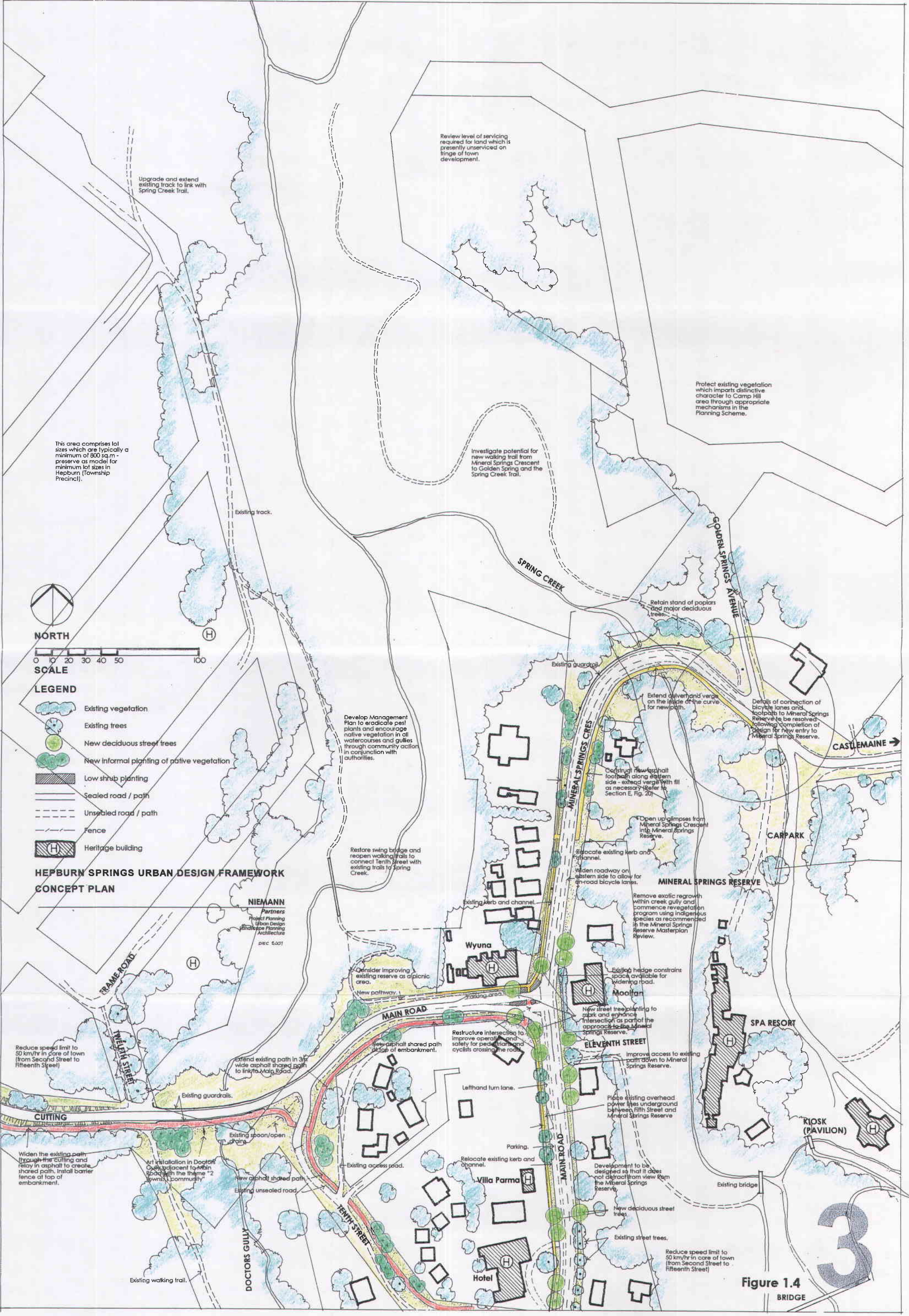
- Existing vegetation
- Existing trees
- New deciduous street trees
- New informal planting of native vegetation
- Low shrub planting
- Sealed road / path
- Unsealed road / path
- Fence
- Heritage building

HEBBURN SPRINGS URBAN DESIGN FRAMEWORK
CONCEPT PLAN

NIEMANN
Partners
Project Planning
Urban Design
Landscape Planning
Architecture
DEC. 2001

Figure 1.3

2



This area comprises lot sizes which are typically a minimum of 800 sq.m - preserve as model for minimum lot sizes in Hephburn (Township Precinct).

Upgrade and extend existing track to link with Spring Creek Trail.

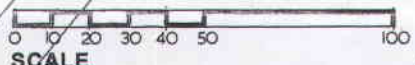
Review level of servicing required for land which is presently unserviced on fringe of town development.

Investigate potential for new walking trail from Mineral Springs Crescent to Golden Spring and the Spring Creek Trail.

Protect existing vegetation which imparts distinctive character to Camp Hill area through appropriate mechanisms in the Planning Scheme.



NORTH



SCALE

LEGEND

- Existing vegetation
- Existing trees
- New deciduous street trees
- New informal planting of native vegetation
- Low shrub planting
- Sealed road / path
- Unsealed road / path
- Fence
- Heritage building

HEPBURN SPRINGS URBAN DESIGN FRAMEWORK
CONCEPT PLAN

NIEMANN
Partners
Project Planning
Urban Design
Landscape Planning
Architecture
ptc 5001

Develop Management Plan to eradicate pest plants and encourage native vegetation in all watercourses and gullies in conjunction with authorities.

Restore swing bridge and reopen walking trails to connect Tenth Street with existing trails to Spring Creek.

Consider improving existing reserve as a picnic area.
New pathway.

Restructure intersection to improve operational and safety for pedestrians and cyclists crossing the road.

Extend driveway and verge on the inside of the curve for new path.

Construct new asphalt footpath along eastern side - extend verge with fill as necessary (Refer to Section E, Fig. 20)

Open up glimpses from Mineral Springs Crescent into Mineral Springs Reserve.

Relocate existing kerb and channel.
Widen roadway on eastern side to allow for on-road bicycle lanes.

Remove exotic regrowth within creek gully and commence revegetation program using indigenous species as recommended in the Mineral Springs Reserve Masterplan Review.

Existing hedge constrains space available for widening road.

New street tree planting to park and enhance intersection as part of the approach to the Mineral Springs Reserve.

Improve access to existing path down to Mineral Springs Reserve.

Relocate existing kerb and channel.
Parking.

Development to be designed so that it does not detract from view from the Mineral Springs Reserve.

New deciduous street trees.
Existing street trees.

Details of connection of bicycle lanes and footpath to Mineral Springs Reserve to be resolved following completion of design for new entry to Mineral Springs Reserve.

MINERAL SPRINGS RESERVE

SPA RESORT

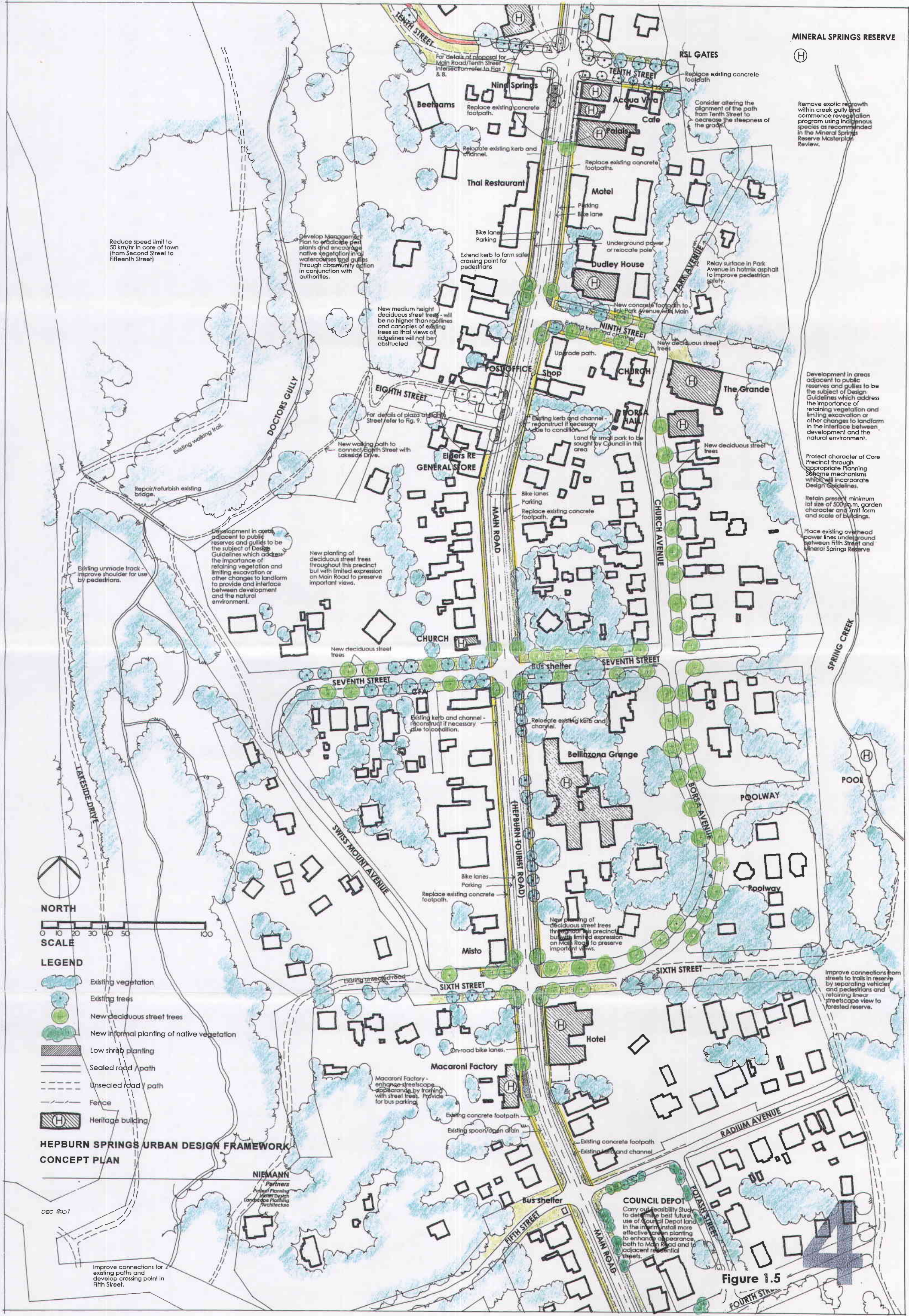
KIOSK (PAVILION)

Reduce speed limit to 50 km/hr in core of town (from Second Street to Fifteenth Street)

Figure 1.4



BRIDGE



Reduce speed limit to 50 km/hr in core of town (from Second Street to Fifteenth Street)

Develop Management Plan to eradicate pest plants and encourage native vegetation in all watercourses and gullies through community action in conjunction with authorities.

New medium height deciduous street trees - will be no higher than rooflines and canopies of existing trees so that views of gullies will not be obstructed

New walking path to connect Eighth Street with Lakeside Drive.

Development in areas adjacent to public reserves and gullies to be the subject of Design Guidelines which address the importance of retaining vegetation and limiting excavation or other changes to landscape to provide and interface between development and the natural environment.

New planting of deciduous street trees throughout this precinct but with limited expression on Main Road to preserve important views.

New deciduous street trees

Existing kerb and channel - reconstruct if necessary due to condition.

Relocate existing kerb and channel.

New planting of deciduous street trees throughout this precinct but with limited expression on Main Road to preserve important views.

Remove exotic regrowth within creek gully and commence revegetation program using indigenous species as recommended in the Mineral Springs Reserve Masterplan Review.

Development in areas adjacent to public reserves and gullies to be the subject of Design Guidelines which address the importance of retaining vegetation and limiting excavation or other changes to landscape in the interface between development and the natural environment.

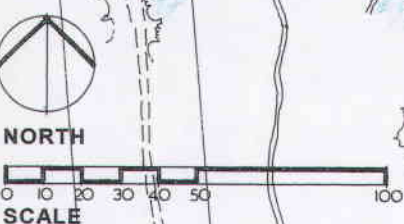
Protect character of Core Precinct through appropriate Planning Scheme mechanisms which will incorporate Design Guidelines.

Retain present minimum lot size of 500sq.m. garden character and limit form and scale of buildings.

Place existing overhead power lines underground between Fifth Street and Mineral Springs Reserve

Improve connections from streets to trails in reserve by separating vehicles and pedestrians and retaining linear streetscape view to forested reserve.

Improve connections for existing paths and develop crossing point in Fifth Street.



- LEGEND**
- Existing vegetation
 - Existing trees
 - New deciduous street trees
 - New informal planting of native vegetation
 - Low shrub planting
 - Sealed road / path
 - Unsealed road / path
 - Fence
 - Heritage building

HEPBURN SPRINGS URBAN DESIGN FRAMEWORK CONCEPT PLAN

NIEMANN Partners
Project Planning
Urban Design
Landscape Planning
Architecture

DEC 2007

Figure 1.5

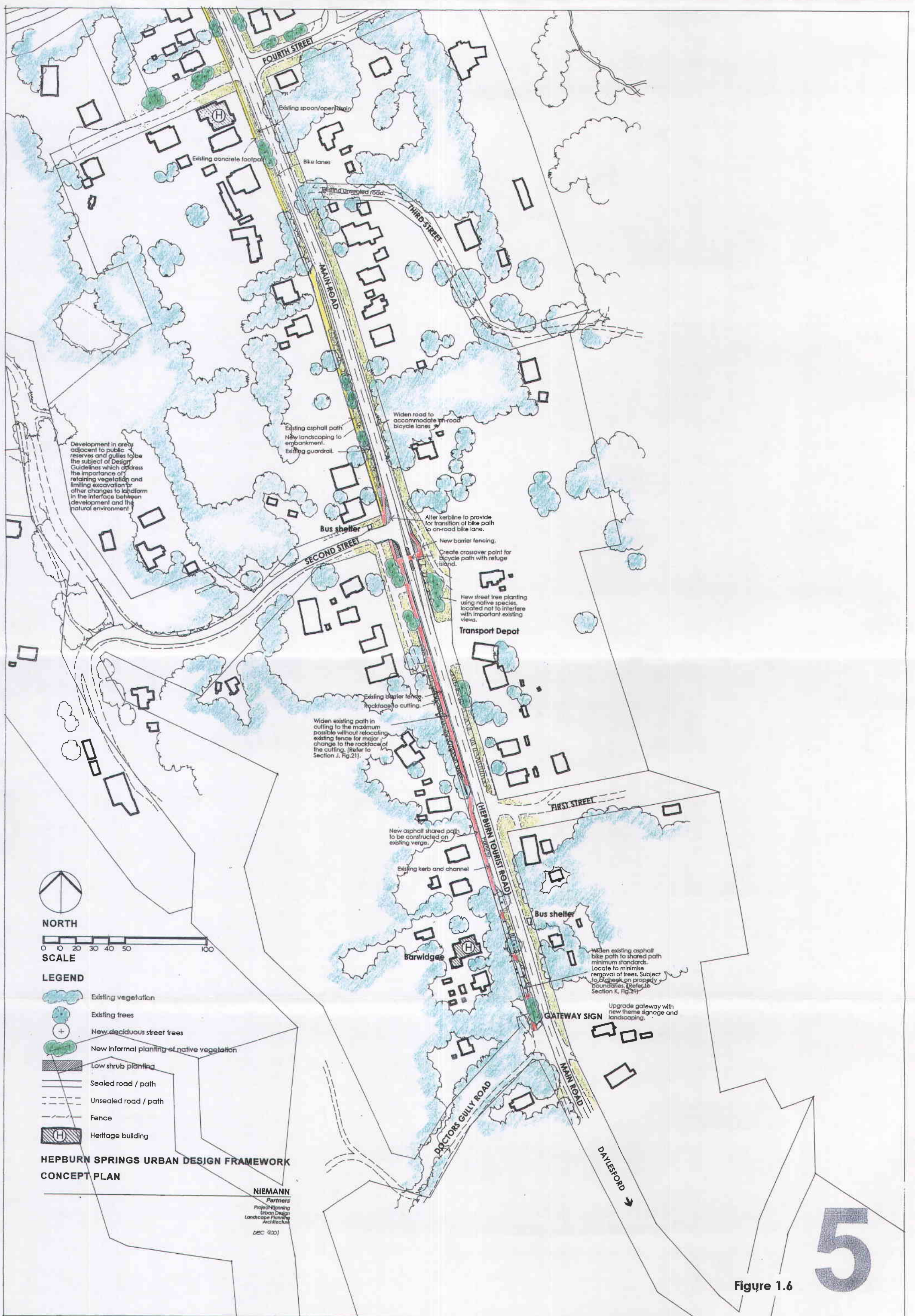


Figure 1.6

□ DETAILED PROPOSALS

TENTH STREET INTERSECTION

(Refer Figs 1.7 and 1.8)

The location of Tenth Street and the adjacent attractors, including the Palais, Cosy Corner Cafe, RSL Memorial Gates and the Springs Hotel, offer the best opportunity to develop a physical central focus for Hepburn Springs. This is the point at which the annual parade and other community activities are concentrated. At these times Tenth Street (east) is closed to vehicles and turned over to a temporary public, or town, square.

As with most of Hepburn Springs, there is insufficient space to create a permanent town square and the design, therefore, focuses on creating smaller areas which will allow casual meetings of small groups of people. Small areas will be created on the south-east and south-west corners of the intersection by extending paving out to the alignment of the bike lanes. The pavement will be in situ concrete with slabs finished in an exposed aggregate to express the Springs theme. Seating will be provided and shade will be available from existing deciduous trees or shop verandahs. A drinking fountain, reflecting the water pump theme, could also be incorporated.

New street trees, which enhance the existing avenue of trees in Tenth Street, will be planted to visually draw attention to the intersection.

During the consultation process there was considerable discussion regarding this proposal. The majority of the community agreed that the location was a valid place for creating a community focal point. There were concerns about current uses of the intersection for U-turns, and also the visual impact and safety issues surrounding the proposed stone bollards.

The final design responds to these concerns by:

- deleting the bollards (they were included to mark the pedestrian/vehicle interface for safety of the pedestrians and could still be installed if necessary).
- ensuring that the junction of the footpath and the road pavement is a "soft" edge and has no kerb and only a small difference in level.
- discouraging U-turns by education, constraining the available road pavement, upgrading Tenth Street to provide a loop via the Newstead Road and extending the bus route to the Hepburn Mineral Springs Reserve.

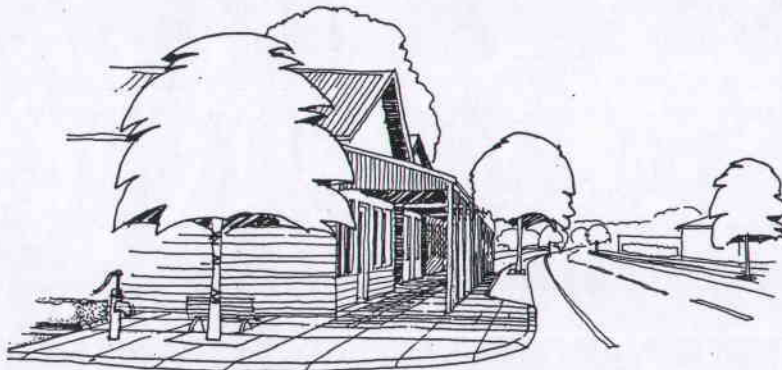


Figure 1.7 - Sketch of Intersection Treatment

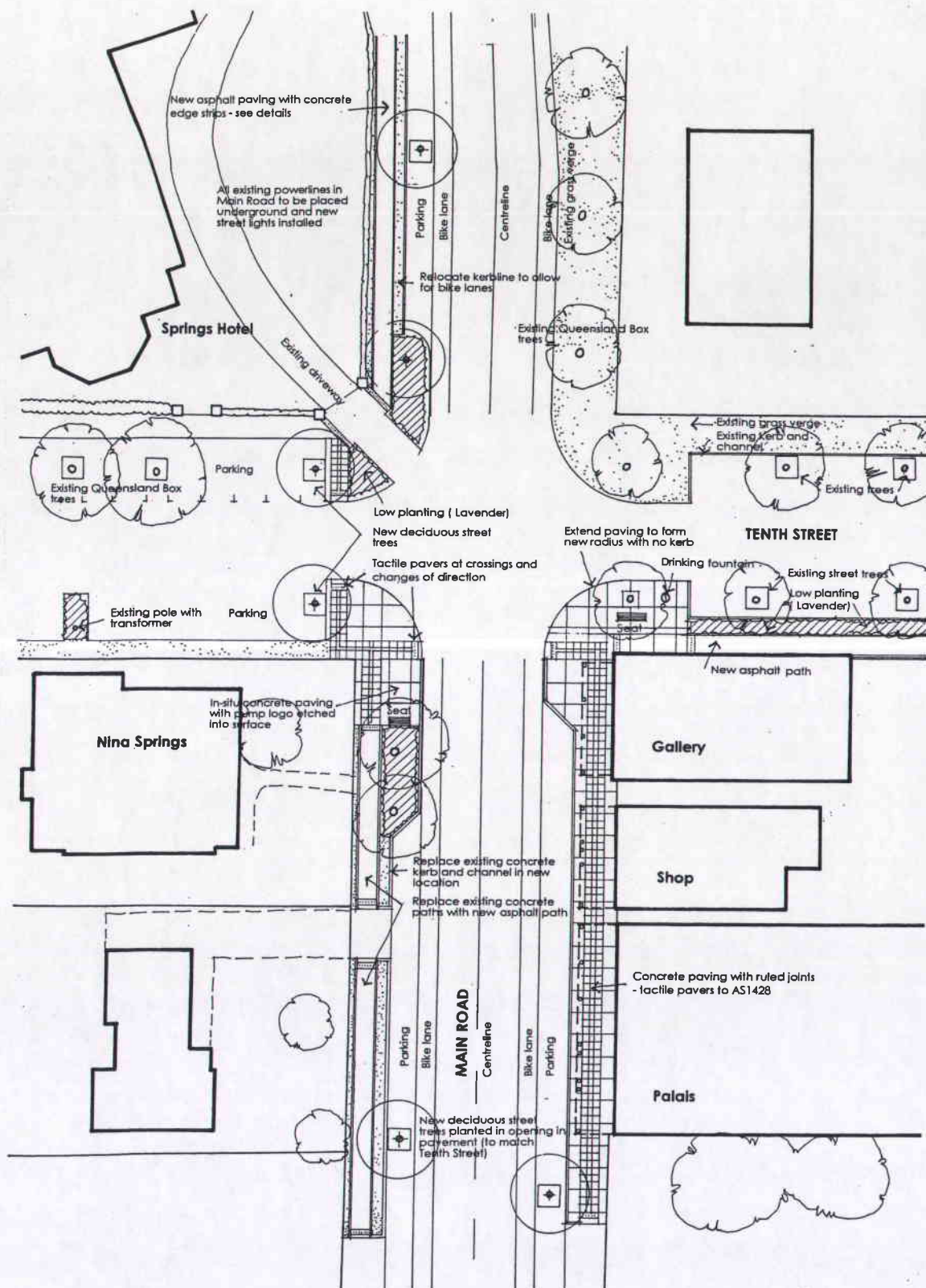


Figure 1.8 - Plan for Intersection for Main Road and Tenth Street

EIGHTH STREET PLAZA

(Refer Fig 1.9)

A small plaza can be created at the intersection of Eighth Street and Main Road by relocating the pavement entry to Eighth Street to the south side of the road reserve. This will leave a small area on the north side which can be paved and developed as a seating area and casual meeting place. This location is between the post office and the general store, and after the Tenth Street intersection, is the second most natural location for a physical community "place". Whilst the area is not large, it will provide a physical focus at a point where community members regularly stop to collect mail, shop, etc. Because of the location of the general store it is also a point at which visitors are likely to stop and, therefore, the civic guide, which is presently located outside the Thai restaurant, could be relocated to this plaza and the opportunity also taken to add facilities for community information and promotion.

If no other options are available for a small park in Main Road, this area will at least meet the need for a meeting place. Public toilets could be incorporated into the space, but the area is small and it may be preferable to install toilets at another location in Main Road if the opportunity arises.

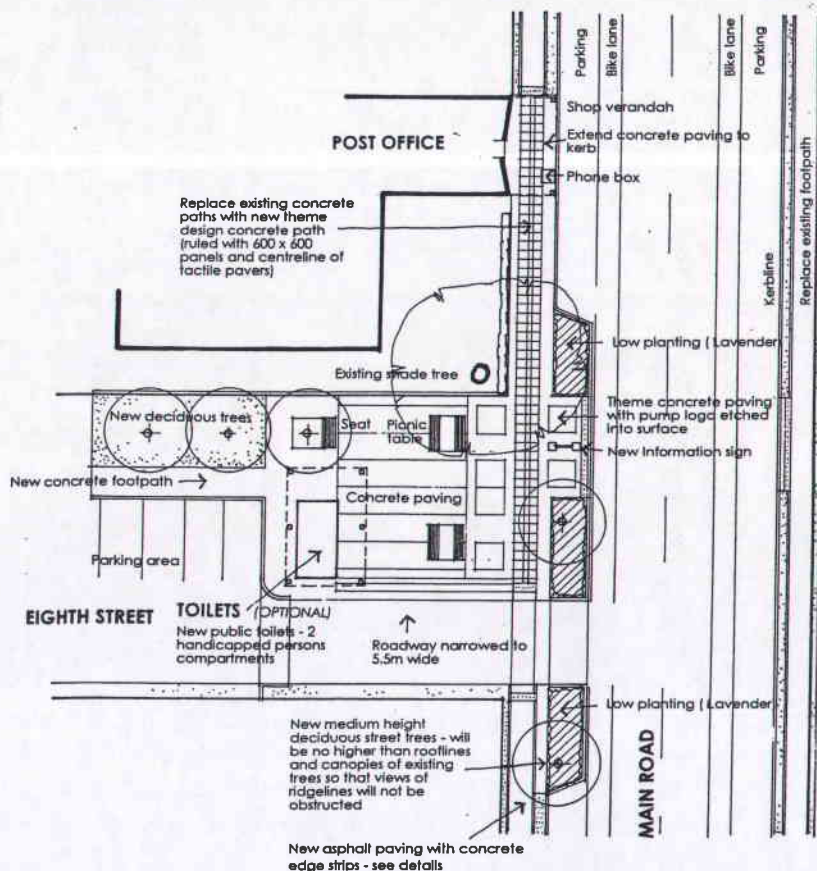


Figure 1.9 - Eighth Street Plaza

ENTRY TO MINERAL SPRINGS RESERVE

At the time of preparing this report, detailed proposals for the revised entry to the Mineral Springs Reserve were not available. These streetscape concepts cannot be completed with any confidence until the entry concept is developed.

Traffic considerations need to be designed sensitively to avoid spoiling the simple character of this section of the streetscape. Provision for pedestrians to cross the road and (ultimately) join a new path following Spring Creek to the north, and vehicles turning in and out of the Hepburn Mineral Springs Reserve, could result in a complex layout which appears heavily engineered.

MATERIALS

Footpaths

Footpaths within Main Road will be 1500mm asphalt paths similar to the existing section between Doctors Gully Road and Third Street. For better definition of the path edges to assist partially-sighted people it is wise to create a contrast in colour along the edge. This can be achieved by line marking, but a concrete edge strip is the preferred method. Shared pathways will also be asphalt (3000mm wide).

In key locations in the core of the township, paved areas will be constructed in concrete. However, in situ techniques and not individual slabs, like the existing paths, are preferred. The latter are subject to displacement by tree roots and for other reasons, causing an uneven level and safety issues. The in-situ concrete will have ruled joints, creating small panels of a similar scale to the size of the existing slabs. In selected locations, the Mineral Springs icon of a hand water pump which is used universally in signage, could be imprinted into the concrete by creating different textures in the concrete surface. In these instances the ruled pattern in the concrete may also be varied to draw attention to the icon. The Victorian Mineral Springs Reserves Masterplan recommends the use of exposed aggregate pavements and the street footpaths could use a similar technique and finish in conjunction with the Mineral Springs icon. In this event, the aggregate should complement the material used in the Mineral Springs Reserve.

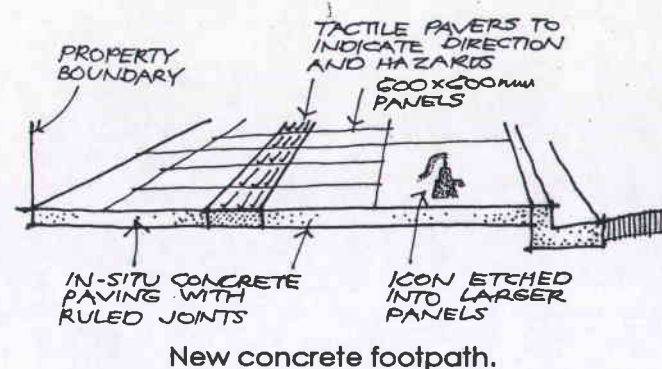
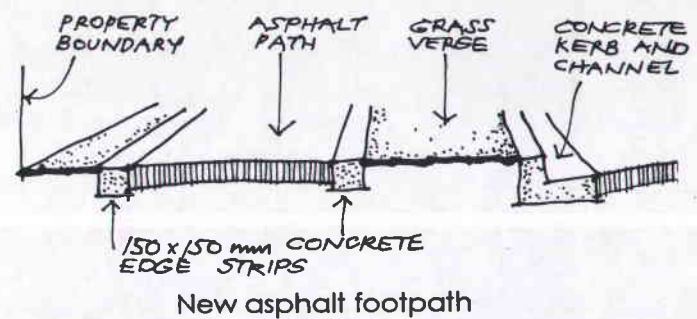


Figure 1.10 - Paving details

Tactile pavers in accordance with AS 1428 should be included at hazard points such as intersections, driveway crossovers and changes of direction.

Walking trails in the reserves will be in accordance with Parks Victoria standards and the Victorian Mineral Springs Reserves Masterplan, which recommends the use of local gravel materials with sufficient fines to allow the gravel to be adequately compacted.

Footpaths outside the Core Precinct will remain as concrete to match existing where replacement or minor extensions are required. New footpaths, eg the extension to Twentieth Street, will be in asphalt with a concrete edge strip to provide colour contrast.

Kerbs and Channels

New kerb and channel sections will only to be used to replace existing where necessary. No additional kerb and channel is proposed except where required for adequate drainage. Concrete to match existing profiles will always be used. Exposed aggregate finishes may be appropriate in some locations to match paving. Elsewhere, roadside drainage will comprise open spoon drains without kerbs, using a grassed surface or sealed finish to blend with the unsealed shoulder. In low impact areas, spoon drains could be formed with a grassed swale.

SIGNAGE

Signage comprises four groups:

- Gateway/town identification signs
 - Directional signage
 - Interpretative and informative signage
 - Advertising/promotional signage.
- This section deals with the first three groups.

The last group is generally part of private development and should be subject to existing Council policies. Detailed building siting and design guidelines could include further advice on acceptable signage.

Gateways

The existing gateways from Daylesford, Castlemaine and Newstead are marked by standard Shire town identification sign boards - the Castlemaine sign being quite utilitarian-looking. The more recent boards will be reused in an attractive landscaped setting, incorporating natural stone and native vegetation. If possible, some artificial lighting thrown from a ground level source and sustained by a solar panel, should be incorporated.

The locations will generally remain as they exist, although on the Daylesford approach, the existing "Hepburn Springs 2 km" sign should be removed and the gateway sign relocated to the western side of the road where it will be more obvious.

The Castlemaine Road sign will be replaced by the newer style sign.

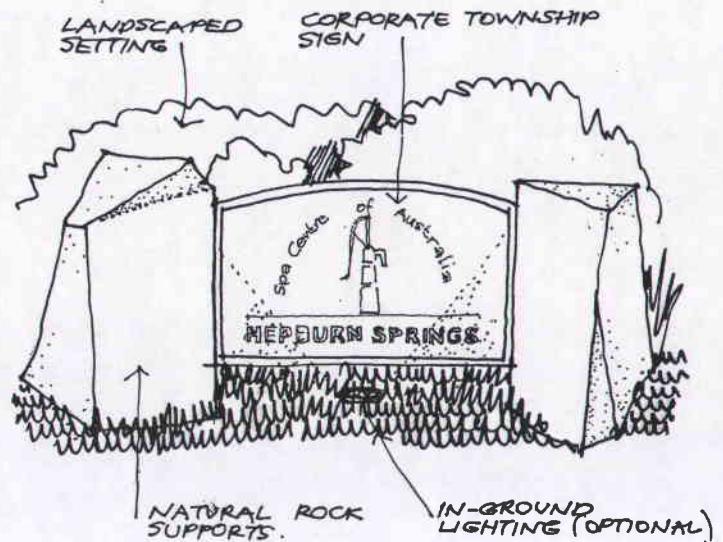


Figure 1.11 - Gateway Sign

Directional Signage

Directional signage provides information for motorists, cyclists and pedestrians. The needs of all these groups are usually met by street signs but, if walking (and to a lesser extent, cycling), is to be encouraged, some smaller-scale, additional signage is recommended.

Large VicRoads panel signs should not be used unless absolutely necessary, and then they should be the minimum height and areas allowed by VicRoads' standards. They should be sensitively located so they do not obstruct views to landmark sites or significant landscapes, nor visually compete with those elements.

Signs at intersections should be minimised to display essential directional information only. Tourist and accommodation pointer signs should be kept to a minimum and displayed separately.

Wherever possible, the amount of street signage should be minimised and that which is necessary should be amalgamated on one panel. Essential directional signage should be separate from promotional signage, such as the blue signs used for accommodation. One concept that has been seen elsewhere is to categorise accommodation by areas and use different coloured arrows on street signs to direct visitors to the general location where the blue signs could provide specific directions. Notwithstanding, some major attractions are deserving of individual signage.

A glaring example of poor street signage occurs at the intersection of the Hepburn Tourist Road and Newstead Road.

This includes:

- a large VicRoads sign south of the intersection which is insensitively positioned with respect to Villa Parma
- a multitude of signs on the island in the intersection, which convey too much information and only add to visual clutter in the streetscape
- a group of signs adjacent to the Mooltan hedge facing traffic in Newstead Road.

This instance provides a case study for detailed consideration of more appropriate sign design and location.

The signs adjacent to the Mooltan hedge were altered some time between September 2000 and August 2001 to display the "Springs" in the Sienna tourism colour. Unfortunately, the change replaced on sign panel with three separate panels. The poles supporting the panels are also very obvious as their silver colour contrasts with the dark green of the cypress hedge.

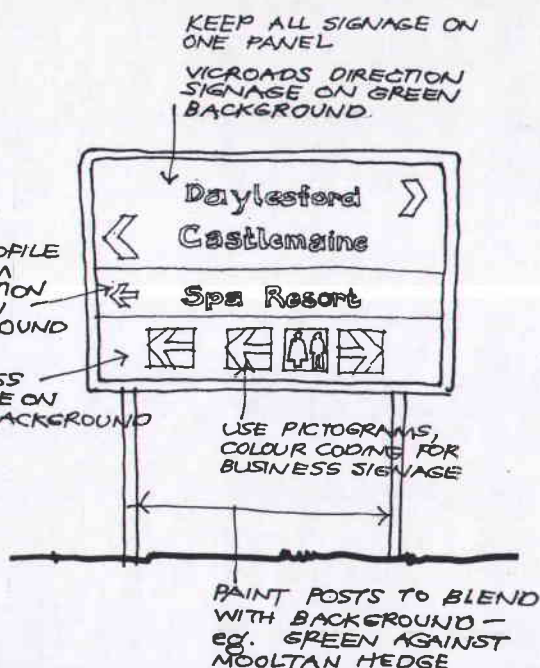


Figure 1.12 - Rationalising Road Signs

At the pedestrian level, signage along footpaths and trails should be provided specifically for their information. This could include directions to specific locations (Post Office, school, Hepburn Mineral Springs Reserve, springs, etc) as well as distance and walking time. This type of signage exists in the reserves but would be valuable in some Main Road locations, such as Tenth Street. Two design options are proposed.

The first is to mount pointer signs on the post of new lighting standards to be used to illuminate pedestrian areas, such as at the Tenth Street intersection. The light fittings will match those already installed in the Hepburn Mineral Springs Reserve. Lighting would operate from a metered supply.

The second option is to erect panels at key locations. These will display various destinations on one panel which will be supported by a rock structure.

Icons could be used instead of words for directions to toilets, identified walking routes, (eg heritage trail), and other facilities. Walking trails should be accompanied by a pamphlet and interpretive signs at places of interest. A good example of an interpretive sign is the panel at the Blowhole, erected by Parks Victoria.

Information sign

The primary location for tourism information is near Eighth Street (General Store and Post Office). A sign panel, incorporating the Civic Guide, could be installed as part of the plaza proposal. (The Civic Guide is presently outside the Thai Restaurant).

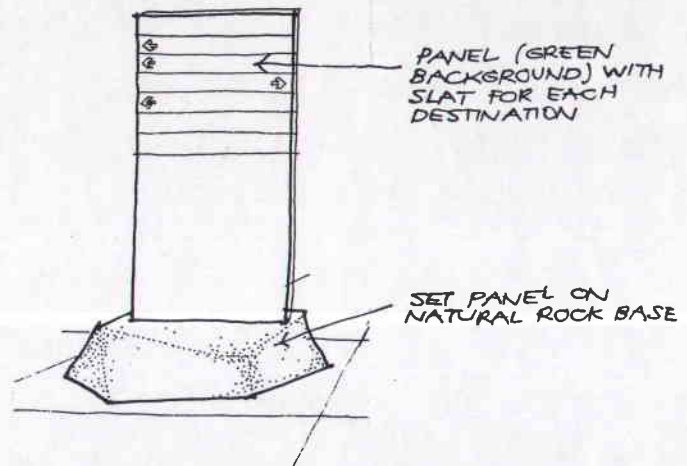
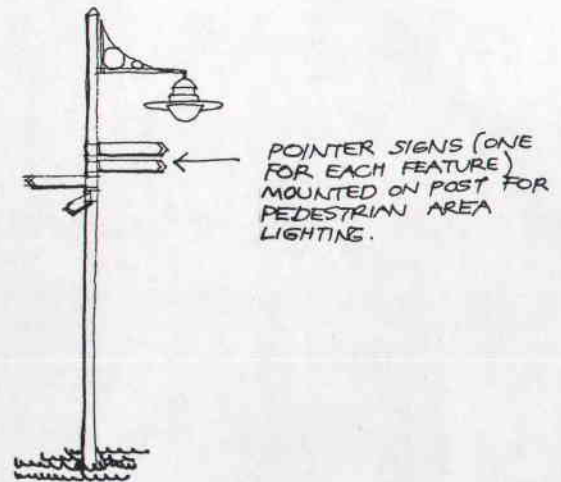


Figure 1.13 - Directional Signage

STREET FURNITURE

The major elements of street furniture are seats, picnic tables, litter bins, street lights, tree guards, bike racks, bollards and bus shelters.

Seats

The type proposed for general use is a Standard Melbourne City Council design with a steel frame and timber slats. It is proposed for use in the Hepburn Mineral Springs Reserve. It is also suitable for use by the elderly and infirm as it has arms. In the event that public art projects are initiated as recommended elsewhere, this design could be replaced by single one-off seats designed by artists at key locations.

Seats should be provided at no more than 300m intervals (preferably every 200m) along Main Road, and positioned so that they (or outstretched legs) do not obstruct adjacent paths.

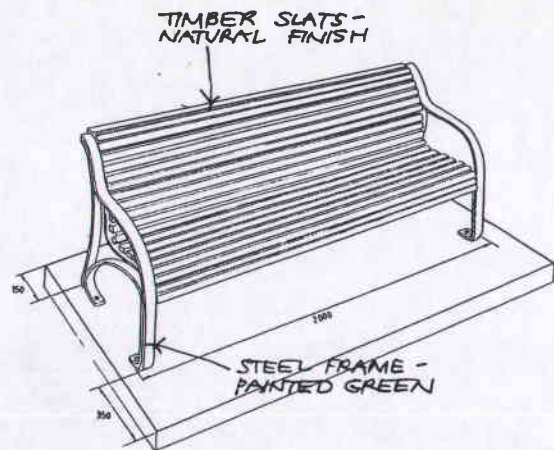


Figure 1.14 - Proposed Seat

Tables

These should match the seat design. The design should be high enough and free of obstructions from legs to allow wheelchair use.

Litter bins

Ideally litter bins that matched the seats in style and materials would be used, however, the Shire uses wheeled bins by preference. If possible, these should be enclosed in a structure with a steel frame and steel or timber slats.

Street lights

As previously mentioned, street lights located to illuminate high volume pedestrian areas could match those already used in the Hepburn Mineral Springs reserve. They would be located at the Tenth Street intersection and the Eighth Street plaza, as well as the new public park in Main Road.

Tree guards

Advanced deciduous trees should be staked and tied when planted and until they are established. Tree guards are not considered essential for long-term protection of trees in Hepburn Springs, but the method of staking could provide a simple design feature in the short term.

Bike Racks

A simple, painted galvanised pipe style is proposed.

Bollards

Bollards are not proposed. If they become necessary in the future, either a simple, painted post (150 x 150mm) with a chamfered pyramid top, or a split stone design, could be used. A precast concrete design with an irregular form and exposed aggregate finish could be substituted for stone if necessary.

Bus shelters

The location of shelters will be dependent on the future of the bus service(s). The existing stone structure at the corner of Fifth Street should be used as a model for all new shelters. Existing shelters should be repainted in accordance with preferred colour scheme.

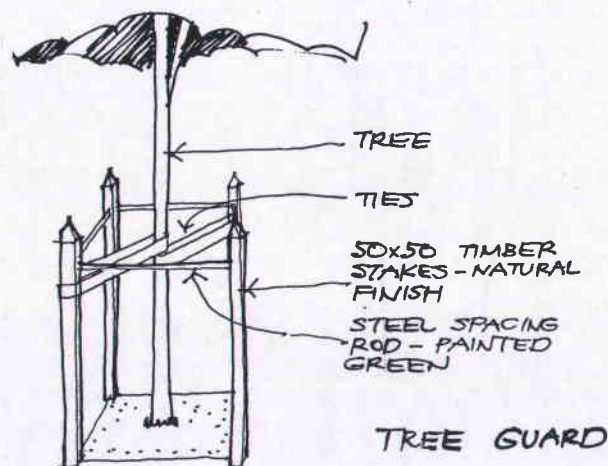
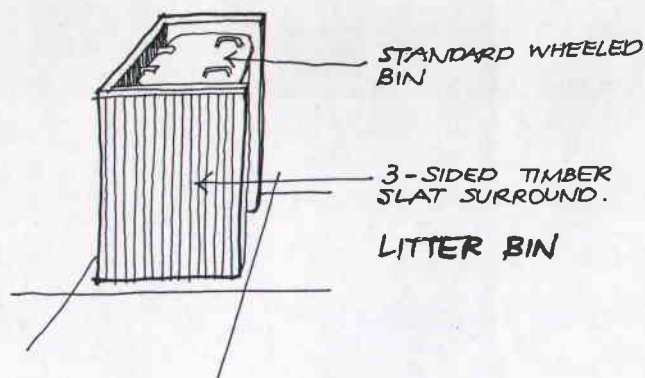
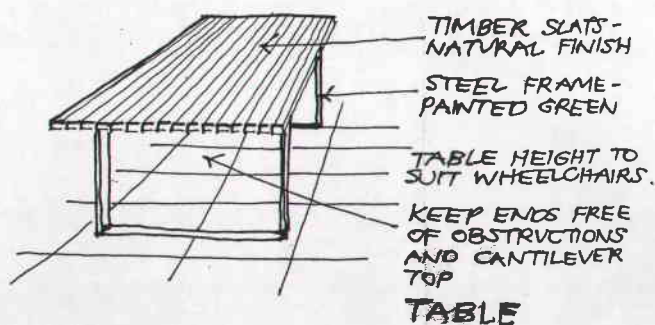


Figure 1.15 - Street Furniture

COLOUR SCHEME

Existing installations, and proposals for the Hepburn Mineral Springs Reserve, provide the basis for colours to be used in signage and street furniture. The scheme should be selected from:

- Indian Red - used in Hepburn Corporate signage
- Cream - used in Hepburn Corporate signage
- Brunswick Green - used in existing light standards in Hepburn Mineral Springs Reserve
- Deep blue - proposed for the steel frames of seats in Hepburn Mineral Springs Reserve.

The deep blue colour was recommended for the Hepburn Mineral Springs Reserve in the Masterplan Review because it is a "symbolic of the water as the unifying element of these diverse reserves". It could also be used as a theme colour in streetscapes throughout Hepburn Springs. This would give Hepburn Springs streetscapes an image which was distinctly different from Daylesford.

However, it is felt that Brunswick Green is more compatible with the corporate colours of Indian Red and Cream. Green could become the dominant colour in the streetscape, rather than the red and cream colours that tend to prevail in Daylesford. Green is also compatible with the forested natural setting of Hepburn Springs.

Suggested colour scheme:

- Gateway signage - red and cream graphics with green posts
- Street furniture - green steel frame, clear/stained natural timbers
- Street lights (pedestrian areas) - green
- Directional signs - green and cream, except for:
 - signage for Hepburn Mineral Springs Reserve and other springs which could incorporate the deep blue.
 - signs to heritage places or historic sites which could be red and cream.
- Bus shelters - timber trim should be green and cream.
- Street signs - continue new red and cream sign panels - paint posts green in Main Road (Core Precinct).

Heritage buildings are subject to paint colour controls. This colour scheme is compatible with those controls.

□ MINOR STREETS

Streetscape proposals for the minor streets generally match the proposals for Main Road, where appropriate. New footpaths will generally be required to strengthen the pedestrian links between Main Road and the reserves. These should be constructed in asphalt, although in some locations, concrete may be needed to match existing paths.

Entrances to the reserves may be defined by a simple arrangement of timber posts, with chains or steel boom gates to restrict vehicle access, if needed.

HEPBURN SPRINGS URBAN DESIGN FRAMEWORK

PART 3 - IMPLEMENTATION

PRIORITIES

The recommendations of the Urban Design Framework have been assembled to form a series of projects, each capable of being implemented in its own right. A priority has been assigned to each project:

VERY HIGH

HIGH

MEDIUM

LONG TERM

Priorities reflect the key issues which were established with the community early in the project, subject to some rationalisation to overcome conflicts for instance, undergrounding of powerlines has a high priority, however, it is unrealistic to plan for early implementation due to lack of funding.

The proposed works program generally follows the priority order, however, it is possible that some of the higher order projects will take longer to achieve and some lesser order works will proceed first because they are more immediately achievable.

Some projects will be carried out in stages, and in these cases have been listed according to the priority of their most important parts. The staging will be determined by the priority of individual parts of the proposals and availability of funding.

COST ESTIMATES

Broad cost estimates for most of the proposals in the Urban Design Framework have been prepared and are included in the following Summary of Projects. The basis for cost estimating is necessarily broad and the suggested budgets are intended for long term financial planning only. Prior to adopting its budget for each financial year, the Council should carry out more detailed planning of each project to be included in that year's work, in order to confirm current estimates. This will require developing proposals in more detail in nearly all instances.

Cost estimates expressed in this report are inclusive of design costs and contingency allowances where appropriate but exclude GST. They are at 2001 rates and should be inflated when they are reviewed for Council budgets in the future.

FUNDING

Hepburn Shire Council is the prime source of funding for works within Hepburn Springs. However, other sources of funding may be available and will be needed to implement the recommendations.

The township community has the potential to be a major resource through the provision of voluntary labour and fund raising, although care must be taken to set realistic expectations of what can be achieved.

Alternative funding authorities often respond favourably to proposals that can demonstrate a cooperative effort between local government, community groups, and, if possible, private industry. A small amount of Council funding, combined with volunteer effort from the township

community and the support of private industry through resources and/or financial contributions, may be a successful formula for gaining matching funding from State and/or Federal agencies.

Projects that are well developed in conjunction with the community also have a better chance of receiving external funding than those that are only in the early stages of planning.

Funding agencies include:

- Heritage Victoria (Department of Infrastructure)
 - Funding for Heritage Studies
 - Heritage Fund - assists owners of significant heritage places with finance for conservation works
- Department of Premier and Cabinet - Community Support Fund
 - Sport and Recreation projects
 - Promotion of the Arts
 - Promotion of Tourism
- Business Victoria - Partnerships for Growth and other programs
- Powerlines Relocation Committee (Victoria)
- Parks Victoria
- Department of Natural Resources and Environment
 - Conservation Project Grants
 - Volunteers and Friends Groups support
- VicRoads (including Bicycle Victoria)
- Natural Heritage Fund
- Philanthropic organisations
- Commercial sponsorship

Federal and State employment schemes also offer alternative funding opportunities and tax incentives are available for conservation works to registered heritage places.

Government funding is subject to frequent policy changes and the Council should ensure that it keeps abreast of current funding programs so that it can maximise its opportunities.

□ SUMMARY OF PROJECTS

The following Summary lists the projects, grouped in priority order, that arise from the recommendations of the Urban Design Framework.

For planning purposes, a time frame is proposed for each project, based on the Council's financial year. At present, the program extends over 5 years, but due to funding shortages and other constraints, it is expected to take longer. The Summary is designed to be regularly reviewed and updated by Council - it is not a static document.

SUMMARY OF PROJECTS

RECOMMENDATION / PROJECT	PROPOSED ACTION	BY	SUGGESTED BUDGET	YEAR	OTHER POSSIBLE FUNDING SOURCES	COMMENTS
VERY HIGH PRIORITIES						
Construct or upgrade the following paths as a priority <ul style="list-style-type: none"> Existing footpaths on both sides of Main Road between Tenth Street and Fifth Street, and particularly those sections in poor condition Existing footpath on south side of Tenth Street leading to the RSL Memorial gates 	<ul style="list-style-type: none"> Incorporate in bike path project - see below. Incorporate in bike path project - see below. 	Refer Bike Paths below Refer Bike Paths below	Refer Bike Paths below \$7,500	2001/2002 2002/2003 2001/2002	VicRoads	VicRoads funding confirmed for bike paths (see below) Do in conjunction with Main Road project
Construct the proposed bike path between Daylesford and Tenth Street	<ul style="list-style-type: none"> Prepare a detailed streetscape plan for Main Road Appoint consultant to prepare detailed streetscape design for Main Road. Consult community/ VicRoads Prepare construction documents Call tenders/ appoint contractor Construct. 	Council/ consultant/ contractor	\$900,000	2001/2002 2002/2003	VicRoads	Streetscape design will integrate bike path with total streetscape concept, including street trees, street furniture, signage, paving and street lighting (latter subject to undergrounding of power lines).
Change to the default 50km/hr speed limit in Main Road	<ul style="list-style-type: none"> Seek VicRoads approval Change signage 	Council/ VicRoads	\$1000	2001/2002	VicRoads	
Prepare a detailed Township Character Strategy including: <ul style="list-style-type: none"> a character study a detailed land suitability analysis a landscape analysis a viewshed analysis 	<ul style="list-style-type: none"> Prepare project brief Call for submissions Appoint consultant Prepare strategy (and consult community) Adopt strategy and program Implement recommendations. 	Council/ consultant	\$20,00 Character Study only \$20,000	2003/2004 2004/2005	DOI	Include building siting and design guidelines. May have to be done in stages, depending on funding. ??

RECOMMENDATION / PROJECT	PROPOSED ACTION	BY	SUGGESTED BUDGET	YEAR	OTHER POSSIBLE FUNDING SOURCES	COMMENTS
Increase screen planting on all street frontages of the Depot.	<ul style="list-style-type: none"> • Prepare design • Carry out planting 	Council	\$10,000 (contractor)	2002/2003	Community may provide volunteer labour and substantially reduce cost.	Could be included in Main Road Streetscape design
HIGH PRIORITIES						
Adopt a 500 m ² minimum lot size for residential development	<ul style="list-style-type: none"> • Prepare Planning Scheme amendment and exhibit • Seek DOI approval. 	Council/ DOI	NIL (in-house)	2002/2003		This decision will have implications for the whole Shire.
Include the Urban Design Framework as a Referenced Document in the Planning Scheme to provide an Interim Policy for Township Character	<ul style="list-style-type: none"> • Prepare Planning Scheme amendment and exhibit • Seek DOI approval 		NIL (in-house)	2002/2003		
Review the policy statements of the MSS regarding the nature and extent of commercial uses in the town centre	<ul style="list-style-type: none"> • Prepare Planning Scheme amendment and exhibit • Seek DOI approval 		NIL (in-house)	2002/2003		
Investigate models for a community consultative body to develop an effective management strategy for environmental management of the public reserves	<ul style="list-style-type: none"> • Research other models • Draft model • Consult community and authorities • Adopt 	Council/ DNRE/ Parks Vic/ CFA/CMA/ VMWC/ community	NIL (in-house)	2002/2003		
Investigate the most appropriate planning scheme controls to create buffer zones at the interface between developed township land and the public reserves	Include in brief for Township Character Strategy	Council/ consultant	Included in budget for Township Character Study)	2003/2004		
Construct the following paths for pedestrians and /or cyclists: <ul style="list-style-type: none"> • Shared path along Newstead Road from Doctors Gully to Hepburn Tourist Road and Mineral Springs Crescent 	<ul style="list-style-type: none"> • Seek additional VicRoads funding for extensions to the Main Road bicycle path • Prepare schematic design • Consult community/VicRoads • Prepare detailed design 	Council/ contractor	\$50,000	2004/2005	VicRoads	Combine as one project with bike path extensions if budget allows

RECOMMENDATION / PROJECT	PROPOSED ACTION	BY	SUGGESTED BUDGET	YEAR	OTHER POSSIBLE FUNDING SOURCES	COMMENTS
<ul style="list-style-type: none"> Footpath along Main Street and Mineral Springs Crescent from Tenth Street to Mineral Springs Reserve New shared path to school frontage to ensure continuity of path along Main Road (including upgrading of path from Tenth Street to shared path). New shared path along Tenth Street from Main Road to Newstead Road 	<ul style="list-style-type: none"> Call tenders/appoint contractor construct 		\$50,000 \$90,000 \$45,000	2004/2005 2005/2006 2005/2006	VicRoads VicRoads VicRoads	
Install bicycle racks at key locations in the town	Place orders and install according to streetscape plan	Council	Cost included in streetscape improvements or \$4000 as separate project.	2003/2004		
Alter the intersection of the Newstead Road and Hepburn Tourist Road to improve pedestrian safety and streetscape appearance.	<ul style="list-style-type: none"> Prepare schematic design Consult community/VicRoads Prepare detailed design Call tenders/appoint contractor construct 	Council/contractor	\$25,000	2004/2005	VicRoads	.Includes changes to signage.
Install on-road bicycle lanes from Tenth Street to the Mineral Springs Reserve	<ul style="list-style-type: none"> Seek additional VicRoads funding for extensions to the Main Road bicycle path Prepare schematic design Consult community/VicRoads Prepare detailed design Call tenders/appoint contractor construct 	Council/contractor	\$15,000	2004/2005	VicRoads	Install in conjunction with footpath project

RECOMMENDATION / PROJECT	PROPOSED ACTION	BY	SUGGESTED BUDGET	YEAR	OTHER POSSIBLE FUNDING SOURCES	COMMENTS
Develop a priority program for repair, upgrading and installation of walking trails in the reserves in conjunction with Parks Victoria, VMWC, and DNRE	Convene meeting to discuss.	Council/ DNRE/ Parks Vic/ VMWC	Unknown	2003/2004	Parks Victoria Regional Tourism Program	Cost for upgrade determined by program.
Increase the frequency of the bus service and extend the journey	<ul style="list-style-type: none"> Explore options with bus service operator 	Council/ operator	NIL	2002/2003		
Provide long term parking for buses and large vehicles	<ul style="list-style-type: none"> Explore options Consult operators 	Council	NIL	2002/2003		
Review and update the Conservation/Heritage Study for the Shire	<ul style="list-style-type: none"> Prepare project brief Call for submissions Appoint consultant Prepare strategy (and consult community) Adopt strategy and program Implement recommendations. 	Council/ contractor	\$20,000	2003/2004	Heritage Victoria	
Restore the swing bridge in Doctors Gully	<ul style="list-style-type: none"> Prepare scope of works Call tenders/appoint contractor Carry out work 	Council/ Parks Vic	Unknown	2005/2006	Regional Tourism Program Community Support Fund	Not supported by Parks Vic. Dependent on external funding.
Consider appropriate measures in the Planning Scheme to ensure protection of significant vegetation	Incorporate in brief for Township Character Strategy	Council/ Consultant	Refer Township Character Strategy	2003/2004		Include in brief for Township Character Strategy.
Clear vegetation around Jacksons Lookout to improve views	<ul style="list-style-type: none"> Discuss with Parks Victoria Carry out work 	Parks Vic	Unknown	2002/2003	Parks Victoria	May be ongoing need to clear vegetation - subject to Parks Victoria agreement
Improve views into HMSR from Main Road and the Mineral Springs Crescent, by clearing vegetation on public land	<ul style="list-style-type: none"> Establish scope of works Carry out work 	Council/ VMWC	Unknown	2003/2004		Part of HMSR Masterplan implementation
Develop a program of streetscape improvements with a common design theme	<ul style="list-style-type: none"> Appoint consultant to prepare program Prepare design manual. 	Council/ consultant	\$5000 (program and design manual)	2002/2003 (program only)		Assumes design themes set by Main Road Streetscape plan.. Includes street furniture and new signage.

RECOMMENDATION / PROJECT	PROPOSED ACTION	BY	SUGGESTED BUDGET	YEAR	OTHER POSSIBLE FUNDING SOURCES	COMMENTS
Investigate potential availability of land for a small park in the town centre	<ul style="list-style-type: none"> Identify potential sites and approach owners 	Council	NIL (in-house)	2002/2003+		
Investigate issues relating to the use of the Primary School grounds for general public use	Consult school	Council	NIL (in-house)	2002/2003		
Investigate alternative sites for the Depot operation	Appoint consultant to prepare feasibility study	Council/consultant	\$5,000	2003/2004		
MEDIUM PRIORITIES						
Build on the programs of the MFPP with regard to public awareness and education about fire risk and safety standards by preparing information sheets	<ul style="list-style-type: none"> Review available information Prepare information sheets 	Council/ CFA/DNRE/ Parks Vic/ MFPC	\$5000	2004/2005	CFA/DNRE/Parks Vic.	
Review the mapping of WMO areas	Convene discussion with CFA	CFA/ Council	NIL (CFA)	2002	CFA	May already be underway - supports MFPP program
Investigate possible improvements to the paths in the HMSR between Main Road / Tenth Street and Ninth Street and the Mineral Springs Reserve to reduce the steepness of the grade to the limits acceptable in AS 1428 or other feasible standards	<ul style="list-style-type: none"> Appoint access consultant Prepare report on feasibility Prepare scope of recommended works 	Council/ VMWC/ consultant	\$5000	2003/2004		Extend project implementing HMSR Masterplan
Provide more publicity and information about walking routes	<ul style="list-style-type: none"> Appoint consultant Prepare pamphlet 	Council/ consultant	\$5000	2003/2004	Regional Tourism Program.	
Extend the Main Road footpath and bike path / on-road lanes from Fourteenth Street to Breakneck Gorge.	<ul style="list-style-type: none"> Prepare schematic design Consult community/VicRoads Prepare detailed design Call tenders/appoint contractor Construct 	Council/ consultant	\$125,000	2005/2006 2006/2007	VicRoads	Could be broken into 2 stages (at Twentieth Street)

RECOMMENDATION / PROJECT	PROPOSED ACTION	BY	SUGGESTED BUDGET	YEAR	OTHER POSSIBLE FUNDING SOURCES	COMMENTS
Review the extent of trails through the public reserves that Parks Victoria and DNRE will allow to be used for mountain biking	Convene meeting to discuss	Council/ Parks Vic/ DNRE/ VMWC	NIL	2002/2003		
Investigate alternatives to the conventional bus service	Prepare report on options for Council consideration	Council	NIL	2003/2004		
Upgrade Ajax Road/Fifteenth Street as a by-pass route	<ul style="list-style-type: none"> • Prepare schematic design • Consult community/VicRoads • Prepare detailed design • Call tenders/appoint contractor • Construct 	Council	\$325,000	2006/2007		
Develop a heritage walking trail with signage and a guide pamphlet	<ul style="list-style-type: none"> • Appoint consultant • Prepare pamphlet 	Council/ community	N/A	2003/2004	Regional Tourism Program	Could be combined with other publicity projects
Establish a program encouraging the community to plant street trees	<ul style="list-style-type: none"> • Prepare detailed proposals • Canvass community interest 	Council/ community	N/A	2003/2004		
Provide public toilets within the Main Road Core Precinct	Action dependent on available site	Council	\$50,000	2004/2005	Community Support Fund	Preferred location in Main Road - timing dependent on land availability.
Consider possible amendments to local policies in the Planning Scheme regarding the level of servicing that should be required in Residential 1 land.	<ul style="list-style-type: none"> • Convene discussion with authorities • Prepare report to Council 	Council/ DNRE/CMA/ VMWC	NIL (in-house)	2002/2003		Include in brief for Township Character Study
Establish a community art panel	<ul style="list-style-type: none"> • Prepare terms of reference and membership guidelines • Advertise for expressions of interest • Appoint panel 		NIL	2002+		
Seek expressions of interest from local artists and artisans for the design and provision of street furniture	<ul style="list-style-type: none"> • Prepare scope • Canvass interest 	Council	N/A	2002/2003	Arts Victoria	Cost of implementation dependent on scope of works.

RECOMMENDATION / PROJECT	PROPOSED ACTION	BY	SUGGESTED BUDGET	YEAR	OTHER POSSIBLE FUNDING SOURCES	COMMENTS
Seek to have the SPPF policies expanded to include water conservation objectives and policies	<ul style="list-style-type: none"> • Prepare submission to DOI • Seek community input • Submit to DOI 	Council/ DOI	NIL (in-house)	2002/2003		
LONG-TERM PRIORITIES						
Produce an information pamphlet to increase awareness of design issues	<ul style="list-style-type: none"> • Appoint consultant • Prepare pamphlet 	Council/ Consultant	\$5000	2005+		Base on recommendations of Township Character Study
Seek opportunities to incorporate sufficient open space for a small park in new developments in or near the town centre	<ul style="list-style-type: none"> • Prepare guidelines • Canvass with prospective development applicants 	Council	NIL	on-going		High need.
Underground all overhead cabling in Main Road and Mineral Springs Crescent between Fifth Street and the entry to Hepburn Springs Mineral Springs Reserve	<ul style="list-style-type: none"> • Ask Powercor and cable television service providers for estimates and technical input. • Seek funding from Powerlines Relocation Committee • Prepare detailed design • Council or Powercor to appoint contractor • Construct. 	Council/ Powercor	\$800,00- \$1,000,000	Dependent on funding	Victorian Powerlines Relocation Committee (\$1:\$1)	Ideally this project should precede bike paths project, however, funding will not be available in the near future. Should remain a high priority with timing being governed by funding.
OTHER RECOMMENDATIONS						
Strictly enforce the Planning Scheme requirements for carparking in new development. Do not allow concessions for available kerbside parking	Enforcement policy	Council	NIL	on-going		
Establish Design Review panel	<ul style="list-style-type: none"> • Report to Council on format, terms of reference etc. • Seek expressions of interest • Appoint Panel 	Council	NIL	on-going		